

# Project Agreement with respect to Kāpiti Expressway:

## Process for Revocation of State Highway 1

NZ Transport Agency (NZTA)

Kāpiti Coast District Council (KCDC)



**PROJECT AGREEMENT WITH RESPECT TO PROCESS FOR REVOCATION OF  
STATE HIGHWAY ONE - KĀPITI EXPRESSWAY**

Date: this 7<sup>th</sup> day of August 2012

**PARTIES**

**NZ Transport Agency** a Crown Entity established under section 93 of the Land Transport Management Act 2003, and its successors at law (*NZTA*).


**Kāpiti Coast District Council** a territorial authority, and its successors at law (*KCDC*).

**DEFINITIONS**

**Date of revocation**, if the Chief Executive of the Ministry of Transport (the *Secretary*) makes a decision to revoke the State highway status of the relevant State highway, means the date at which any such revocation takes effect as specified in any *Gazette* notice revoking the State highway status.

**Fit for Purpose** means that at the date of revocation the revoked section of State highway 1 will provide a level of service equivalent to other roads providing a similar function within the local road network and be safe.

**BACKGROUND**

- A The NZTA wishes to construct, operate and maintain the MacKays to Peka Peka section (*Kāpiti Expressway*) of the Wellington Northern Corridor Road of National Significance (*Wellington RoNS*).
- B The Kāpiti Expressway forms an approximately 16 kilometre length section of the Wellington RoNS. It covers the improvements planned for the State highway corridor from Raumati at the southern end, continuing through Paraparaumu and Waikanae to Peka Peka in the north.
- C The Kāpiti Expressway will predominantly follow the path of the previous but now withdrawn Sandhills Motorway designation and will deliver:
- a four lane highway with four full or half interchanges at Poplar Avenue, Kāpiti Road, Te Moana Road, and Peka Peka Road respectively,
  - construction of new local roads and access roads to retain local connectivity, and
  - an additional crossing of the Waikanae River.
- D The NZTA lodged a Notice of Requirement for a designation and resource consent applications for the Kāpiti Expressway with the Environmental Protection Authority (*EPA*) on 20 April 2012.
- E The NZTA has the sole powers of control of the State highway network.
- 



- F KCDC has the sole powers of control of the local road network.
- G Once the Kāpiti Expressway has been constructed, the section of what is currently State highway 1 from Poplar Avenue to Peka Peka Rd may no longer be required for State highway purposes. This section of road is more particularly defined as route position 1012/0.00 to route position 1023/5.00.
- H NZTA proposes to make a recommendation to the Secretary in relation to whether the State highway status should be revoked in respect of that section of State highway 1 from Poplar Avenue to Peka Peka Rd, subject to the final decision and any conditions imposed on any designation and resource consents, if granted, with respect to the Kāpiti Expressway.
- I The effect of any revocation is that the Poplar Avenue to Peka Peka Rd section of State highway 1 would become a local road for which KCDC would be responsible.
- J The parties agreed to a series of objectives as the basis for advancing consideration of the Kāpiti Expressway under an Alliance structure. These objectives include:
- The nature and scale of the existing State Highway 1, especially at Waikanae and Paraparaumu town centres, shall deliver a viable and attractive roading and access system for local needs; that is, is able to accommodate the impacts of projected passenger transport movement and growth, vehicle, pedestrian and cycle movements and enhances those town centres.*
- K Before making a recommendation to the Secretary, NZTA is required to consult with a number of people, including KCDC.
- L The MacKays to Peka Peka Expressway Alliance completed a SH1 (Kāpiti) Reconfiguration Scheme Design Report, including indicative costings, dated 11 November 2011, following a formal public consultation process with the Kāpiti community in August 2011. The indicative costings have been updated by the Alliance - the latest version dated 31 July 2012 is attached as Appendix One.

**THE PARTIES AGREE** as follows:

**1 Parties' Objectives in Undertaking this Agreement**

**1.1** The parties' objectives as they relate to this Agreement are to:

- (a) document the works which would be 100% funded by the NZTA as part of the Kāpiti Expressway project being works for the purpose of rendering the Poplar Avenue to Peka Peka Road section of State highway 1 fit for purpose at the time of any revocation of State highway status (*Category 1 works*). This includes documenting the agreed future form and function of the Poplar Avenue to Peka Peka Road section of State highway 1 (which is relevant to determining whether or not any such works would render it "fit for purpose"),



- (b) document the agreed range of works to achieve a future form and function of the Poplar Ave to Peka Peka Rd section of SH1 that if undertaken by KCDC, KCDC would apply to NZTA for part funding (Category 2 works);
- (c) document those activities that if undertaken by KCDC would be fully funded by KCDC (*Category 3 works*), and
- (d) agree a process for the NZTA making a recommendation as to whether or not to revoke the State highway status of the existing State highway 1.

## 2 Objectives in ensuring "fit for purpose"

2.1 The parties agree that the following objectives apply to ensuring the Poplar Avenue to Peka Peka Rd section of State highway 1 is 'fit for purpose' at the time of any revocation.

- (a) Affordable – have achieved value for money;
- (b) Risk-based – have provided a resilient network;
- (c) Integrated and optimised - the Poplar Avenue to Peka Peka Road section of State highway 1 shall function as an effective part of the district and regional transport network; and
- (d) Future-proofed - the Poplar Avenue to Peka Peka Road section of State highway 1 shall maintain its ability to operate as an emergency bypass route.

2.2 The parties agree that the following objectives apply to the ultimate form and function of the Poplar Avenue to Peka Peka Rd section of State highway 1 if its State highway status is revoked:

- (a) That it provide a nature and scale, especially at Waikanae and Paraparaumu town centres, which delivers a viable and attractive roading and access system for local needs;
- (b) That it is able to accommodate the impacts of projected passenger transport movement and growth, vehicle, pedestrian and cycle movements; and
- (c) That it enhances the Waikanae and Paraparaumu town centres.

## 3 Parties Obligations

3.1 The parties recognise that the NZTA and KCDC have functions and obligations:

- (a) Arising from their respective statutory roles and responsibilities;



- (b) In the case of the NZTA, including, but not limited to, under the Land Transport Management Act 2003 (*LTMA*) and the Government Rooding Powers Act 1989; and
- (c) In the case of the KCDC, to the ratepayers and residents of the Kapiti Coast District.

3.2 The parties' obligations under this Agreement are conditional on:

- (a) Resource Management Act 1991 (*RMA*) approvals required for the Kāpiti Expressway being granted; and
- (b) The NZTA deciding, in its sole discretion, to proceed with construction of the Kāpiti Expressway.

3.3 The physical works described in this Agreement will only be given effect to once construction of the Kāpiti Expressway has commenced.

3.4 The NZTA's obligations under this Agreement are subject to NZTA's statutory obligations and any conditions imposed on any designation and resource consents granted with respect to the Kāpiti Expressway.

3.5 KCDC's obligations in relation to any matter under any revocation process are subject to KCDC's statutory obligations.

3.6 The parties agree to carry out any actions or make any decisions required under this Agreement in a timely and reasonable manner.

#### **4 Development of Underpinning Design Framework**

4.1 The Alliance has undertaken as part of the MacKays to Peka Peka Expressway Project the development of design concepts for the current State highway 1 between Poplar Avenue and Peka Peka should the Kāpiti Expressway project proceed with some of those design concepts only applying if the State highway status of that section of State highway 1 is revoked. The index of the associated documentation can be found in Appendix Two.

4.2 The parties note that the following are the formally adopted objectives of the Alliance's SH1 Reconfiguration workstream which guided the development of the design concepts as set out in Appendix Two:

##### **(a) Traffic Function**

Retain the current State highway 1 as the main spine road for the local road network and allow it to function as a national distributor road in the event of an emergency.

##### **(b) Local Planning**

Maintain economic viability of the District's existing major town centres as social, employment, retail and transport nodes.



(c) Safety

Exhibit best practice in the design of the local network including the existing State Highway and be consistent with urban design standards.

(d) Urban form

Accommodate the impacts of projected passenger transport movement and growth, vehicle, pedestrian and cycle movements and enhance those town centres.

(e) Strategy

Achieve consistency with the District's Development Management Strategies and associated regulatory policy (including the District Plan and Plan Changes 79 and 80).

(f) Value for Money

Deliver project solutions that show value for money and are consistent with NZTA's funding requirements and mitigation requirements.

4.3 The parties also note that the form and function of the section of State highway 1 running from Poplar Avenue to Peka Peka Road, should its State highway status be revoked, will be determined by applying the following criteria:

- (a) Identifying the network function of the road (at the time of handover) based on the existing local road hierarchy and surrounding land use, the KCDC sustainable transport strategy and asset management plan needs;
- (b) Identifying the level of service that will reasonably meet the network function of the road, as defined in the relevant asset management plans;
- (c) Undertaking a safety assessment to ensure that change in function will not create a safety defect that did not exist before; and
- (d) Identifying whether the road needs to fulfil functions beyond the functions it would be required to fulfil solely as a local road.

## 5 Three Step Process for Funding of Revocation Package

5.1 Both parties will follow the process outlined in Appendix Three.

### Step 1: Allocation of Funding Contributions

5.2 On the basis of the SH1 (Kāpiti) Reconfiguration Scheme Design Report, dated 11 November 2011, which included indicative costings, and the Outline Specification in Appendix Four, the NZTA and KCDC have agreed to the allocation of funding responsibility, under three categories:



- (a) Activities funded at 100% by NZTA and undertaken by the NZTA as part of the Kāpiti Expressway project (*Category 1 Works*). The following works along the Poplar Avenue to Peka Peka Road section of State highway 1 will be assessed as part of the delivery of the Kāpiti Expressway project:
    - (i) Carriageway modification including kerbs, medians, pavement removal and stabilisation and service relocation to achieve a safe urban street and rural road environment to the satisfaction of KCDC and the NZTA,
    - (ii) Intersection improvements to achieve safe intersections to the satisfaction of the KCDC and the NZTA, and
    - (iii) All temporary traffic management activities associated with the Kāpiti Expressway Project including the reconfiguration of State highway 1.
  - (b) Activities that may be part funded by NZTA and part funded by KCDC in proportions determined by the appropriate financial assistance rate. The following works will be assessed for funding by NZTA at the relevant financial assistance rate (53% at 2012) (*Category 2 Works*):
    - (i) Off road walkway and cycleway works, in the vicinity of the Poplar Avenue to Peka Peka Road section of State highway 1 including pedestrian crossings, lighting and signage which demonstrate safety and transport benefits to the satisfaction of the NZTA and KCDC.
  - (c) Activities in the vicinity of the Poplar Avenue to Peka Peka Road section of State highway 1 fully funded by KCDC (*Category 3 Works*). This includes:
    - (i) Feature landscaping
    - (ii) Tree planting
    - (iii) Street furniture
    - (iv) Bio-retention swales, rain gardens and devices unless demonstrated necessary to treat contaminants arising from new areas of pavement.
- 5.3 For the avoidance of doubt, KCDC will not request the NZTA to fund those items identified under 5.2(c) above.
- 5.4 Any request for NZTA funding of transport/bus shelters will be assessed independently of this agreement and will be considered on its merits.



Step 2: Seeking Package Approval

- 5.5 KCDC agrees that in order for the NZTA to make a decision regarding any funding for the Category 2 Works that NZTA must follow established NZTA processes. The parties agree that NZTA will seek package approval for the Category 2 Works. The parties note that at as at the date of this Agreement that the benefit/cost ratio for the Category 2 Works has been indicatively assessed as 1.1.

Step 3: Funding Approval Sought

- 5.6 The NZTA Highways and Network Operations business unit shall seek indicative funding approval for the construction of the Category 1 Works as part of the funding approval for the construction of the Kāpiti Expressway project.
- 5.7 If the Category 2 Works are endorsed for funding as a package of works by the NZTA Planning and Investment business unit, KCDC will at its discretion seek approval for partial funding of any such package at approximately the time that funding approval is sought for the final design of the Kāpiti Expressway project. This is likely to be in the second quarter of 2013.
- 5.8 The parties note their current intentions that detailed business cases will be submitted for approval of funding for Category 1 and 2 Works prior to the implementation of the Kāpiti Expressway. However, the parties agree that if there is a delay in KCDC submitting a detailed business case for the Category 2 Works that will not prevent NZTA from commencing implementation of the Expressway.

**6 Matters not Addressed Under This Agreement**

- 6.1 The parties agree that the following matters will not be addressed in the context of this Agreement:

- (a) Existing bridges seismic condition

The parties note that the question of the seismic condition of any bridge on the Poplar Avenue to Peka Peka Road section of State highway 1 has not been addressed at the time of signing this Agreement and remains to be identified.

The parties agree that any seismic performance risks will be taken into account by the NZTA in making a recommendation to the Secretary as to whether or not to revoke the State highway status of the Poplar Avenue to Peka Peka Road section of State highway 1.

- (b) Maintenance funding allocations
- (c) Rationalisation of existing RMA Designations
- (d) Disposal of surplus land



## **7 Process for NZTA making a Recommendation**

- 7.1 The NZTA shall make best efforts within 24 months of the Kāpiti Expressway becoming operational to make a recommendation to the Secretary, pursuant to section 103 of the Land Transport Management Act 2003, in relation to whether the State highway status should be revoked in respect of the Poplar Avenue to Peka Peka Road section of State highway 1.
- 7.2 NZTA will keep KCDC informed of the progress of that recommendation and the Secretary's decision.
- 7.3 NZTA will continue to manage and maintain the Poplar Avenue to Peka Peka Road section of State highway 1 to provide the current level of service until such time as the State highway status of this section of State highway 1 may be revoked, at which time this section of State highway 1 shall become local road for which the KCDC shall be responsible.
- 7.4 If the Secretary approves a recommendation to revoke, then the NZTA will:
  - (a) Provide KCDC with all the information held by the NZTA relating to the Poplar Avenue to Peka Peka Road section of State highway 1 within NZTA's asset management system, 10 year programme, plan records, risk register, bylaws, Limited Access Road (LAR) status and other records or databases held by the NZTA; and
  - (b) Develop with the KCDC, the parties' ongoing areas of responsibility (maintenance boundaries) at the interface between the new State highway and local road network.

## **8 KCDC's Obligations**

- 8.1 If the Secretary approves a recommendation to revoke, then the KCDC will:
  - (a) Operate and manage what is currently the Poplar Avenue to Peka Peka Road section of State highway 1 as a major community connector (as set out in the network hierarchy and according to levels of service set out in the relevant asset management plans) after revocation and regulate the land uses in the area to ensure the continuation of this status, and
  - (b) Ensure that the ongoing operation of the major community connector maintains its ability to operate as an emergency bypass route.

## **9 Work Standards**

- 9.1 All Category 1, Category 2 and Category 3 Works will generally comply as applicable with the following documents (or their later versions):
  - (a) KCDC Subdivision and Development – Principles and Requirements 2010 DRAFT



- (b) NZTA Standards
- (c) NZS 4404:2004 Land Development and Subdivision Engineering Standard
- (d) Austroads, Guide to Road Design 2009
- (e) AS/NZS Standards

9.2 The design standards and specifications may vary from that drawn in State highway 1 Reconfiguration (Kāpiti) Scheme Design Report, 11 November and drawings (Volume 3) dated 30 September 2011 and specified in this document but only by agreement between the parties. For further detail see the relevant sections of Appendix Four - State Highway 1 (Kapiti) Reconfiguration - Outline Specification.

9.3 The obligations in clauses 9.1 to 9.2 are subject to any conditions imposed on any relevant statutory approvals for the works.

## 10 Revocation Process

If the Secretary approves a recommendation to revoke the State highway status of the Poplar Avenue to Peka Peka Road section of State highway 1, then:

- 10.1 NZTA will carry out all routine maintenance to provide the current level of service to within two weeks prior to the date of revocation;
- 10.2 KCDC will carry out all routine maintenance from the date of revocation; and
- 10.3 KCDC and the NZTA shall jointly inspect the Poplar Avenue to Peka Peka Road section of State highway 1 to identify any remedial work not completed.

## 11 Limited Access Road Status

- 11.1 The Poplar Avenue to Peka Peka Road section of State highway 1 has limited access road status. If the state highway status is revoked from the this section of State highway 1, then in terms of section 96 of the Government Roding Powers Act 1989 the State highway ceases to be a State highway, and it is deemed to be a limited access road created under the Local Government Act 1974; and the administration of that road shall pass to KCDC.
- 11.2 If KCDC wishes NZTA to remove the limited access road status over any parts of the Poplar Avenue to Peka Peka Road section of State highway 1, then it shall advise the NZTA of this prior to, or during, the consultation process to be carried out pursuant to section 103(8) of the LTMA. KCDC acknowledges that the NZTA would be required to comply with all obligations under the Government Roding Powers Act 1989 in relation to any proposed revocation of limited access road status. If the State highway is still a limited access road at the date of any revocation of State highway status, then responsibility for any revocation of limited access road status of that section of road would rest with KCDC.



## 12 Force majeure

12.1 Neither party shall be liable for any failure or delay in performance under this Agreement to the extent that such failures or delays are proximately caused by conditions beyond the parties' reasonable control which the party claiming the benefit of this force majeure clause is unable to overcome by the exercise of reasonable diligence and at a reasonable cost; including but without limitation:

- (a) An Act of God, fire, earthquake, storm, flood or landslide
- (b) Explosion, public mains electrical supply failure or nuclear accident
- (c) Sabotage, riot, civil disturbance, insurrection, epidemic, national emergency (whether in fact or law) or act of war (whether declared or not),

but does not include any event which the party claiming the benefit of this force majeure clause could have prevented or overcome by exercising a standard of reasonable care or reasonable endeavours.

## 13 Dispute Resolution

13.1 If any dispute arises under this Agreement, the parties agree to comply with the following provisions of this clause before commencing any other form of dispute resolution (including court proceedings).

13.2 Any party claiming that a dispute has arisen under this Agreement between the parties shall give written notice to the other party of the dispute and require both of the parties' representatives to meet together to attempt to settle the dispute. The other party shall, upon receipt of such notice, promptly ensure that its representatives attend such meeting and attempt to resolve the dispute.

13.3 The parties shall use their reasonable endeavours to ensure that the parties' representatives who attend a meeting under clause 13.2 shall, within 10 business days after the giving of the notice, seek to resolve the dispute.

13.4 The parties shall within a further period of 10 business days (or within such longer period as the representatives may agree is appropriate), use their reasonable endeavours to agree, in good faith, on a process for resolving the whole or part of the dispute through means other than litigation or arbitration (including, without limitation, further negotiations, mediation, conciliation or independent expert determination).

13.5 In the unlikely event that agreement is not reached through escalation, the parties shall agree to arbitration on the following basis:

- (a) The arbitration shall be conducted by a sole arbitrator in New Zealand pursuant to the Arbitration Act 1996;



- (b) The parties' respective responsibilities for the costs of the arbitration shall be determined by the arbitrator; and
- (c) The parties shall be bound by the decision of the arbitrator.

Provided always that the arbitrator shall have no jurisdiction in relation to any dispute that concerns the operation (including, without limitation, the exercise or non-exercise) of any statutory function, obligation or power of either the NZTA or KCDC.

- 13.6 The parties acknowledge that any negotiated or arbitrated outcome or outcome arising as a result of an independent expert determination will need to be consistent with any conditions imposed on any designations, resource consents or other statutory authorisations obtained for the Kapiti Expressway and with the statutory functions, obligations and powers of the NZTA and KCDC.

#### 14. Notices

- 14.1 Any notice, demand, consent or other communication (*Notice*) given or made under this Agreement:
  - (a) Must be in writing and signed by a person duly authorised by the sender
  - (b) Must either be delivered to the intended recipient by post or by hand or fax to the address or fax number below of the address or fax number last notified by the intended recipient to the sender



- 14.2 Unless and until the relevant party gives notice of different contact details the parties contact details for the purposes of this Agreement are:

NZTA - State highway Manager – Wellington  
PO Box 5084, Lambton Quay  
Wellington 6145  
**Fax:** 64 4 894 3305


KCDC – Chief Executive  
Kapiti Coast District Council  
Private Bag 60601  
Paraparaumu 5254

## 15 Conditional Agreement

- 15.1 This Agreement is conditional upon the approval of the elected Council of the Kāpiti Coast District Council.

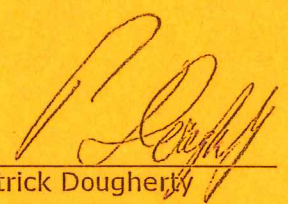
**EXECUTED** as an agreement

Signed on behalf of the  
**NEW ZEALAND TRANSPORT AGENCY**  
by




Rod James  
State Highway Manager, Wellington

Signed on behalf of the  
**KĀPITI COAST DISTRICT COUNCIL**  
by



Patrick Dougherty  
Chief Executive Officer



Lyndon Hammond  
Planning and Investment Manager,  
Central



**Appendix One: SH1 Kapiti Reconfiguration Project Costing Matrix (by Priority Area) 31 July 2012**

Project Details		Project Details		Project Details	
Project Name	Project ID	Project Name	Project ID	Project Name	Project ID
Project A	101	Project B	102	Project C	103
Project D	104	Project E	105	Project F	106
Project G	107	Project H	108	Project I	109
Project J	110	Project K	111	Project L	112
Project M	113	Project N	114	Project O	115
Project P	116	Project Q	117	Project R	118
Project S	119	Project T	120	Project U	121
Project V	122	Project W	123	Project X	124
Project Y	125	Project Z	126	Project AA	127
Project AB	128	Project AC	129	Project AD	130
Project AE	131	Project AF	132	Project AG	133
Project AH	134	Project AI	135	Project AJ	136
Project AK	137	Project AL	138	Project AM	139
Project AN	140	Project AO	141	Project AP	142
Project AQ	143	Project AR	144	Project AS	145
Project AT	146	Project AU	147	Project AV	148
Project AW	149	Project AX	150	Project AY	151
Project AZ	152	Project BA	153	Project BB	154
Project BC	155	Project BD	156	Project BE	157
Project BF	158	Project BG	159	Project BH	160
Project BI	161	Project BJ	162	Project BK	163
Project BL	164	Project BM	165	Project BN	166
Project BO	167	Project BP	168	Project BQ	169
Project BR	170	Project BS	171	Project BT	172
Project BU	173	Project BV	174	Project BW	175
Project BX	176	Project BY	177	Project BZ	178
Project CA	179	Project CB	180	Project CC	181
Project CD	182	Project CE	183	Project CF	184
Project CG	185	Project CH	186	Project CI	187
Project CJ	188	Project CK	189	Project CL	190
Project CM	191	Project CN	192	Project CO	193
Project CP	194	Project CQ	195	Project CR	196
Project CS	197	Project CT	198	Project CU	199
Project CV	200	Project CW	201	Project CX	202
Project CY	203	Project CZ	204	Project DA	205
Project DB	206	Project DC	207	Project DD	208
Project DE	209	Project DF	210	Project DG	211
Project DH	212	Project DI	213	Project DJ	214
Project DK	215	Project DL	216	Project DM	217
Project DN	218	Project DO	219	Project DP	220
Project DQ	221	Project DR	222	Project DS	223
Project DT	224	Project DU	225	Project DV	226
Project DW	227	Project DX	228	Project DY	229
Project DZ	230	Project EA	231	Project EB	232
Project EC	233	Project ED	234	Project EE	235
Project EF	236	Project EG	237	Project EH	238
Project EI	239	Project EJ	240	Project EK	241
Project EL	242	Project EM	243	Project EN	244
Project EO	245	Project EP	246	Project EQ	247
Project ER	248	Project ES	249	Project ET	250
Project EU	251	Project EV	252	Project EW	253
Project EX	254	Project EY	255	Project EZ	256
Project FA	257	Project FB	258	Project FC	259
Project FD	260	Project FE	261	Project FF	262
Project FG	263	Project FH	264	Project FI	265
Project FJ	266	Project FK	267	Project FL	268
Project FM	269	Project FN	270	Project FO	271
Project FP	272	Project FQ	273	Project FR	274
Project FS	275	Project FT	276	Project FU	277
Project FV	278	Project FW	279	Project FX	280
Project FY	281	Project FZ	282	Project GA	283
Project GB	284	Project GC	285	Project GD	286
Project GE	287	Project GF	288	Project GG	289
Project GH	290	Project GI	291	Project GH	292
Project GJ	293	Project GK	294	Project GL	295
Project GM	296	Project GN	297	Project GO	298
Project GP	299	Project GQ	300	Project GR	301
Project GS	302	Project GT	303	Project GU	304
Project GV	305	Project GW	306	Project GX	307
Project GY	308	Project GZ	309	Project HA	310
Project HB	311	Project HC	312	Project HD	313
Project HE	314	Project HF	315	Project HG	316
Project HH	317	Project HI	318	Project HH	319
Project HJ	320	Project HK	321	Project HL	322
Project HM	323	Project HN	324	Project HO	325
Project HP	326	Project HQ	327	Project HR	328
Project HS	329	Project HT	330	Project HU	331
Project HV	332	Project HW	333	Project HX	334
Project HY	335	Project HZ	336	Project IA	337
Project IB	338	Project IC	339	Project ID	340
Project IE	341	Project IF	342	Project IG	343
Project IH	344	Project II	345	Project IH	346
Project IJ	347	Project IK	348	Project IL	349
Project IM	350	Project IN	351	Project IO	352
Project IP	353	Project IQ	354	Project IR	355
Project IS	356	Project IT	357	Project IU	358
Project IV	359	Project IW	360	Project IX	361
Project IY	362	Project IZ	363	Project JA	364
Project JB	365	Project JC	366	Project JD	367
Project JE	368	Project JF	369	Project JG	370
Project JH	371	Project JI	372	Project JH	373
Project JJ	374	Project JK	375	Project JL	376
Project JM	377	Project JN	378	Project JO	379
Project JP	380	Project JQ	381	Project JR	382
Project JS	383	Project JT	384	Project JU	385
Project JV	386	Project JW	387	Project JX	388
Project JY	389	Project JZ	390	Project KA	391
Project KB	392	Project KC	393	Project KD	394
Project KE	395	Project KF	396	Project KG	397
Project KH	398	Project KI	399	Project KH	400
Project KJ	401	Project KK	402	Project KL	403
Project KM	404	Project KN	405	Project KO	406
Project KP	407	Project KQ	408	Project KR	409
Project KS	410	Project KT	411	Project KU	412
Project KV	413	Project KW	414	Project KX	415
Project KY	416	Project KZ	417	Project LA	418
Project LB	419	Project LC	420	Project LD	421
Project LE	422	Project LF	423	Project LG	424
Project LH	425	Project LI	426	Project LH	427
Project LJ	428	Project LK	429	Project LL	430
Project LM	431	Project LN	432	Project LO	433
Project LP	434	Project LQ	435	Project LR	436
Project LS	437	Project LT	438	Project LU	439
Project LV	440	Project LW	441	Project LX	442
Project LY	443	Project LZ	444	Project MA	445
Project MB	446	Project MC	447	Project MD	448
Project ME	449	Project MF	450	Project MG	451
Project MH	452	Project MI	453	Project MH	454
Project MJ	455	Project MK	456	Project ML	457
Project MM	458	Project MN	459	Project MO	460
Project MP	461	Project MQ	462	Project MR	463
Project MS	464	Project MT	465	Project MU	466
Project MV	467	Project MW	468	Project MX	469
Project MY	470	Project MZ	471	Project NA	472
Project NB	473	Project NC	474	Project ND	475
Project NE	476	Project NF	477	Project NG	478
Project NH	479	Project NI	480	Project NH	481
Project NJ	482	Project NK	483	Project NL	484
Project NM	485	Project NN	486	Project NO	487
Project NP	488	Project NQ	489	Project NR	490
Project NS	491	Project NT	492	Project NU	493
Project NV	494	Project NW	495	Project NX	496
Project NY	497	Project NZ	498	Project OA	499
Project OB	500	Project OC	501	Project OD	502
Project OE	503	Project OF	504	Project OG	505
Project OH	506	Project OI	507	Project OH	508
Project OJ	509	Project OK	510	Project OL	511
Project OM	512	Project ON	513	Project OO	514
Project OP	515	Project OQ	516	Project OR	517
Project OS	518	Project OT	519	Project OU	520
Project OV	521	Project OW	522	Project OX	523
Project OY	524	Project OZ	525	Project PA	526
Project PB	527	Project PC	528	Project PD	529
Project PE	530	Project PF	531	Project PG	532
Project PH	533	Project PI	534	Project PH	535
Project PJ	536	Project PK	537	Project PL	538
Project PM	539	Project PN	540	Project PO	541
Project PP	542	Project PQ	543	Project PR	544
Project PS	545	Project PT	546	Project PU	547
Project PV	548	Project PW	549	Project PX	550
Project PY	551	Project PZ	552	Project QA	553
Project QB	554	Project QC	555	Project QD	556
Project QE	557	Project QF	558	Project QG	559
Project QH	560	Project QI	561	Project QH	562
Project QJ	563	Project QK	564	Project QL	565
Project QM	566	Project QN	567	Project QO	568
Project QP	569	Project QQ	570	Project QR	571
Project QS	572	Project QT	573	Project QU	574
Project QV	575	Project QW	576	Project QX	577
Project QY	578	Project QZ	579	Project RA	580
Project RB	581	Project RC	582	Project RD	583
Project RE	584	Project RF	585	Project RG	586
Project RH	587	Project RI	588	Project RH	589
Project RJ	590	Project RK	591	Project RL	592
Project RM	593	Project RN	594	Project RO	595
Project RP	596	Project RQ	597	Project RR	598
Project RS	599	Project RT	600	Project RU	601
Project RV	602	Project RW	603	Project RX	604
Project RY	605	Project RZ	606	Project SA	607
Project SB	608	Project SC	609	Project SD	610
Project SE	611	Project SF	612	Project SG	613
Project SH	614	Project SI	615	Project SH	616
Project SJ	617	Project SK	618	Project SL	619
Project SM	620	Project SN	621	Project SO	622
Project SP	623	Project SQ	624	Project SR	625
Project SS	626	Project ST	627	Project SU	628
Project SV	629	Project SW	630	Project SX	631
Project SY	632	Project SZ	633	Project TA	634
Project TB	635	Project TC	636	Project TD	637
Project TE	638	Project TF	639	Project TG	640
Project TH	641	Project TI	642	Project TH	643
Project TJ	644	Project TK	645	Project TL	646
Project TM	647	Project TN	648	Project TO	649
Project TP	650	Project TQ	651	Project TR	652
Project TS	653	Project TT	654	Project TU	655
Project TV	656	Project TW	657	Project TX	658
Project TY	659	Project TZ	660	Project UA	661
Project UB	662	Project UC	663	Project UD	664
Project UE	665	Project UF	666	Project UG	667
Project UH	668	Project UI	669	Project UH	670
Project UJ	671	Project UK	672	Project UL	673
Project UM	674	Project UN	675	Project UO	676
Project UP	677	Project UQ	678	Project UR	679
Project US	680	Project UT	681	Project UY	682
Project UV	683	Project UW	684	Project UX	685
Project UY	686	Project UZ	687	Project VA	688
Project VB	689	Project VC	690	Project VD	691
Project VE	692	Project VF	693	Project VG	694
Project VH	695	Project VI	696	Project VH	697
Project VJ	698	Project VK	699	Project VL	700
Project VM	701	Project VN	702	Project VO	703
Project VP	704	Project VQ	705	Project VR	706
Project VS	707	Project VT	708	Project VU	709
Project VV	710	Project VW	711	Project VX	712
Project VY	713	Project VZ	714	Project WA	715
Project WB	716	Project WC	717	Project WD	718
Project WE	719	Project WF	720	Project WG	721
Project WH	722	Project WI	723	Project WH	724
Project WJ	725	Project WK	726	Project WL	727
Project WM	728	Project WN	729	Project WO	730
Project WP	731	Project WQ	732	Project WR	733
Project WS	734	Project WT	735	Project WU	736
Project WV	737	Project WW	738	Project WX	739
Project WY	740	Project WZ	741	Project XA	742
Project XB	743	Project XC	744	Project XD	745
Project XE	746	Project XF	747	Project XG	748
Project XH	749	Project XI	750	Project XH	751
Project XJ	752	Project XK	753	Project XL	754
Project XM	755	Project XN	756	Project XO	757
Project XP	758	Project XQ	759	Project XR	760
Project XS	761	Project XT	762	Project XU	763
Project XV	764	Project XW	765	Project XX	766
Project XY	767	Project XZ	768	Project YA	769
Project YB	770	Project YC	771	Project YD	772
Project YE	773	Project YF	774	Project YG	775
Project YH	776	Project YI	777	Project YH	778
Project YJ	779	Project YK	780	Project YL	781
Project YM	782	Project YN	783	Project YO	784
Project YP	785	Project YQ	786	Project YR	787
Project YS	788	Project YT	789	Project YU	790
Project YV	791	Project YW	792	Project YX	793
Project YY	794	Project YZ	795	Project ZA	796
Project ZB	797	Project ZC	798	Project ZD	799
Project ZE	800	Project ZF	801	Project ZG	802
Project ZH	803	Project ZI	804	Project ZH	805
Project Z					



# Revision 18

## INDICATIVE COSTINGS

Item	Priority Area	Work Category	Guiding Objectives For PAB	Net Cost	Gross Cost	Value Management Reduction	100% NZTA Funding	KQDC 47% Cost	Building Unallocated	Funded By Others
<b>Priority Areas A, B &amp; C Total Cost</b>										
A	Priority Areas: Wairarapa Town Centre (including zones 7, 8 & 9)			\$2,906,655	\$4,512,582	\$0	\$2,906,655	\$0	\$0	\$0
A1.0	Carriageway Modification	213231	3d, 4.9b, 10c	\$874,680	\$1,357,941	\$0	\$874,680	\$0	\$0	\$0
A1.1	services relocation	111		\$214,481	\$332,982	\$0	\$214,481	\$0	\$0	\$0
A1.2	pavement reconstruction/new pavement	111		\$59,657	\$92,618	\$0	\$59,657	\$0	\$0	\$0
A1.3	re-surface road pavement	111, 231, 341		\$146,652	\$227,678	\$0	\$146,652	\$0	\$0	\$0
A1.4	new kerbs and line marking	111, 231, 341		\$89,505	\$137,553	\$0	\$89,505	\$0	\$0	\$0
A1.5	Raised Median - Top soil	122, 213, 214		\$127,248	\$197,873	\$0	\$127,248	\$0	\$0	\$0
A2.1	Top soil	214		\$159,085	\$246,979	\$0	\$159,085	\$0	\$0	\$0
A2.2	Grass	214		\$10,518	\$16,325	\$0	\$10,518	\$0	\$0	\$0
A3.1	Temporary Traffic Management	included		\$59,812	\$92,618	\$0	\$59,812	\$0	\$0	\$0
A3.2	Local Accommodation Works	included		\$5,550	\$8,550	\$0	\$5,550	\$0	\$0	\$0
A4.0	Safety Improvement	341	3d, 4.9b, 10c	\$395,888	\$1,506,116	\$0	\$395,888	\$0	\$0	\$0
A4.1	Intersection modifications	122		\$350,000	\$553,375	\$0	\$350,000	\$0	\$0	\$0
A4.2	Signs and lighting	122, 451		\$18,400	\$29,000	\$0	\$18,400	\$0	\$0	\$0
A5.1	Pedestrian crossings	122, 451		\$100,000	\$155,250	\$0	\$100,000	\$0	\$0	\$0
A6.1	Local Accommodation Works	included		\$235,482	\$365,386	\$0	\$235,482	\$0	\$0	\$0
A6.2	New off road cycleway	124		\$8,787	\$13,641	\$0	\$8,787	\$0	\$0	\$0
A6.3	New off road cycleway bridge at Wairarapa Bridge	451, 452		\$82,574	\$128,197	\$0	\$82,574	\$0	\$0	\$0
A7.1	Temporary Traffic Management	included		\$30,500	\$47,361	\$0	\$30,500	\$0	\$0	\$0
A8.0	Landscaping	10c		\$1,138	\$1,767	\$0	\$1,138	\$0	\$0	\$0
A8.1	Ground cover, mass planting and specimen trees (additional cost over grass)			\$440,474	\$683,835	\$0	\$440,474	\$0	\$0	\$0
A8.2a	services relocation	213231		\$368,580	\$572,841	\$0	\$368,580	\$0	\$0	\$0
A8.2b	Main Street Upgrade	451	9b	\$71,484	\$110,994	\$0	\$71,484	\$0	\$0	\$0
A9.1	New walkways in town centres	451		\$381,150	\$591,735	\$0	\$381,150	\$0	\$0	\$0
A9.2	Street furniture	11a		\$20,000	\$31,050	\$0	\$20,000	\$0	\$0	\$0
A9.3	Bio-retention swales and devices	121		\$214,464	\$332,982	\$0	\$214,464	\$0	\$0	\$0
A9.4	Sustainability initiative	4.9b, 9c, 10c		\$1,100,400	\$1,710,372	\$0	\$1,100,400	\$0	\$0	\$0
B	Priority Areas: Paraparaumu Town Centre (including zones 3 & 4)			\$464,524	\$721,173	\$0	\$464,524	\$0	\$0	\$0
B1.0	Carriageway Modification	213231		\$721,173	\$1,100,400	\$0	\$721,173	\$0	\$0	\$0
B1.1	services relocation	111		\$77,522	\$120,353	\$0	\$77,522	\$0	\$0	\$0
B1.2	pavement reconstruction/new pavement	111, 231, 341		\$71,484	\$110,994	\$0	\$71,484	\$0	\$0	\$0
B1.3	re-surface road pavement	111, 231, 341		\$38,084	\$58,626	\$0	\$38,084	\$0	\$0	\$0
B1.4	new kerbs and line marking	122, 213, 214		\$142,678	\$221,508	\$0	\$142,678	\$0	\$0	\$0
B1.5	Raised Median - Top soil	214		\$165,708	\$258,814	\$0	\$165,708	\$0	\$0	\$0
B2.1	Top soil	214		\$11,022	\$17,112	\$0	\$11,022	\$0	\$0	\$0
B2.2	Grass	214		\$133,445	\$207,174	\$0	\$133,445	\$0	\$0	\$0
B2.3	Temporary Traffic Management	included		\$7,700	\$11,700	\$0	\$7,700	\$0	\$0	\$0
B2.4	Local Accommodation Works	included		\$1,328,868	\$2,063,067	\$0	\$1,328,868	\$0	\$0	\$0
B3.0	Safety Improvement	341	4.9b, 10c	\$1,350,000	\$2,063,067	\$0	\$1,350,000	\$0	\$0	\$0
B3.1	Intersection modifications	122		\$1,000,000	\$1,552,500	\$0	\$1,000,000	\$0	\$0	\$0
B3.2	Signs and lighting	122, 451		\$49,143	\$76,294	\$0	\$49,143	\$0	\$0	\$0
B3.3	Pedestrian crossings	122, 451		\$1,804	\$2,847	\$0	\$1,804	\$0	\$0	\$0
B3.4	Local Accommodation Works	included		\$111,995	\$173,868	\$0	\$111,995	\$0	\$0	\$0
B3.5	Wharomaku Stream Bridge	451		\$1,000,000	\$1,552,500	\$0	\$1,000,000	\$0	\$0	\$0
B4.0	Pedestrian crossing @ Anahua St	451		\$645,000	\$1,000,000	\$0	\$645,000	\$0	\$0	\$0
B4.1	New walkway/cycleway bridge near Rimutaka St Rail Bridge	451, 452		\$179,512	\$278,327	\$0	\$179,512	\$0	\$0	\$0
B4.2	Temporary Traffic Management	included		\$6,887	\$10,332	\$0	\$6,887	\$0	\$0	\$0
B4.3	Landscaping	10c		\$814,453	\$1,264,439	\$0	\$814,453	\$0	\$0	\$0
B4.4	Ground cover, mass planting and specimen trees (additional cost over grass)			\$659,612	\$1,024,048	\$0	\$659,612	\$0	\$0	\$0
B4.5	services relocation	213231	9b	\$154,841	\$240,391	\$0	\$154,841	\$0	\$0	\$0
B4.6	Main Street Upgrade	451		\$339,813	\$530,060	\$0	\$339,813	\$0	\$0	\$0
B4.7	New walkways in town centres	11a		\$20,000	\$31,050	\$0	\$20,000	\$0	\$0	\$0
B4.8	Street furniture	121		\$214,464	\$332,982	\$0	\$214,464	\$0	\$0	\$0
B4.9	Bio-retention swales and devices	4.9b, 9c		\$1,100,400	\$1,710,372	\$0	\$1,100,400	\$0	\$0	\$0
B5.0	Sustainability initiative	4.9b, 9c		\$41,531	\$64,524	\$0	\$41,531	\$0	\$0	\$0
C	Priority Areas: Paparua Town Centre (including town entry zones 1-2a)			\$2,923,271	\$4,538,378	\$0	\$2,923,271	\$0	\$0	\$0
C1.0	Carriageway Modification	213231		\$1,323,651	\$2,054,988	\$0	\$1,323,651	\$0	\$0	\$0
C1.1	services relocation	111		\$338,475	\$521,460	\$0	\$338,475	\$0	\$0	\$0
C1.2	pavement reconstruction/new pavement	111, 231, 341		\$136,206	\$211,460	\$0	\$136,206	\$0	\$0	\$0
C1.3	re-surface road pavement	111, 231, 341		\$66,440	\$103,162	\$0	\$66,440	\$0	\$0	\$0
C1.4	new kerbs and line marking	122, 213, 214		\$553,459	\$828,195	\$0	\$553,459	\$0	\$0	\$0
C1.5	Raised Median - Top soil	214		\$38,468	\$59,753	\$0	\$38,468	\$0	\$0	\$0
C2.1	Top soil	214		\$108,587	\$168,491	\$0	\$108,587	\$0	\$0	\$0
C2.2	Grass	214		\$3,977	\$6,175	\$0	\$3,977	\$0	\$0	\$0
C2.3	Temporary Traffic Management	included		\$323,977	\$500,474	\$0	\$323,977	\$0	\$0	\$0
C2.4	Local Accommodation Works	included		\$164,406	\$255,240	\$0	\$164,406	\$0	\$0	\$0
C3.0	Safety Improvement	341	4.9b, 10c	\$213,995	\$332,227	\$0	\$213,995	\$0	\$0	\$0
C3.1	Intersection modifications	122		\$60,500	\$93,346	\$0	\$60,500	\$0	\$0	\$0
C3.2	Signs and lighting	122, 451		\$71,303	\$111,356	\$0	\$71,303	\$0	\$0	\$0
C3.3	Pedestrian crossings	122, 451		\$4,304	\$6,604	\$0	\$4,304	\$0	\$0	\$0
C3.4	Local Accommodation Works	included		\$104,484	\$162,211	\$0	\$104,484	\$0	\$0	\$0
C3.5	New off road cycleway	124, 452		\$104,484	\$162,211	\$0	\$104,484	\$0	\$0	\$0
C3.6	New off road cycleway bridge	451, 452		\$507,647	\$788,122	\$0	\$507,647	\$0	\$0	\$0
C3.7	Ground cover, mass planting and specimen trees (additional cost over grass)			\$507,647	\$788,122	\$0	\$507,647	\$0	\$0	\$0
C3.8	Sustainability initiative	11a		\$463,512	\$719,602	\$0	\$463,512	\$0	\$0	\$0
C3.9	Bio-retention swales and devices	121		\$1,512,602	\$2,318,102	\$0	\$1,512,602	\$0	\$0	\$0
<b>Priority Areas D, E &amp; F Total Cost</b>										
D	Rural zones (zones 5, 6 and 10)			\$1,324,882	\$2,367,348	\$0	\$1,324,882	\$0	\$0	\$0
D1.0	Carriageway Modification	213231	9c	\$760,374	\$1,180,480	\$0	\$760,374	\$0	\$0	\$0
D1.1	services relocation	111		\$112,680	\$174,936	\$0	\$112,680	\$0	\$0	\$0
D1.2	pavement reconstruction/new pavement	111		\$26,438	\$41,044	\$0	\$26,438	\$0	\$0	\$0
D1.3	re-surface road pavement	111, 231, 341		\$0	\$0	\$0	\$0	\$0	\$0	\$0
D1.4	new kerbs and line marking	122, 213, 214		\$83,104	\$129,019	\$0	\$83,104	\$0	\$0	\$0
D1.5	Raised Median - Top soil	214		\$85,502	\$132,451	\$0	\$85,502	\$0	\$0	\$0
D1.6	Top soil	214		\$5,854	\$9,088	\$0	\$5,854	\$0	\$0	\$0
D1.7	Grass	214		\$425,006	\$659,852	\$0	\$425,006	\$0	\$0	\$0
D1.8	Temporary Traffic Management	included		\$18,750	\$29,109	\$0	\$18,750	\$0	\$0	\$0
D1.9	Local Accommodation Works	included		\$45,000	\$69,863	\$0	\$45,000	\$0	\$0	\$0
D2.0	Safety Improvement	341	9c	\$45,000	\$69,863	\$0	\$45,000	\$0	\$0	\$0
D2.1	Intersection modifications	122		\$0	\$0	\$0	\$0	\$0	\$0	\$0
D2.2	Signs and lighting	122, 451		\$0	\$0	\$0	\$0	\$0	\$0	\$0
D2.3	Pedestrian crossings	122, 451		\$531,678	\$800,661	\$0	\$531,678	\$0	\$0	\$0
D2.4	New off road cycleway	124, 452	4.9b, 10c	\$516,625	\$800,661	\$0	\$516,625	\$0	\$0	\$0
D2.5	New off road cycleway bridge	451, 452		\$37,494	\$58,242	\$0	\$37,494	\$0	\$0	\$0
D2.6	Temporary Traffic Management	included		\$37,494	\$58,242	\$0	\$37,494	\$0	\$0	\$0
D2.7	Landscaping	10c		\$87,810	\$136,325	\$0	\$87,810	\$0	\$0	\$0
D2.8	Ground cover, mass planting and specimen trees (additional cost over grass)			\$87,810	\$136,325	\$0	\$87,810	\$0	\$0	\$0
D2.9	Sustainability initiative	11a		\$37,910	\$59,365	\$0	\$37,910	\$0	\$0	\$0
D3.0	Bio-retention swales and devices	121		\$2,710,495	\$4,208,043	\$0	\$2,710,495	\$0	\$0	\$0
E	Rural zones (zones 5, 6 and 10)			\$2,710,495	\$4,208,043	\$0	\$2,710,495	\$0	\$0	\$0
E1.0	Carriageway Modification	222		\$243,152	\$377,493	\$0	\$243,152	\$0	\$0	\$0
E1.1	chip seal suburban pavement (Option A)	222, 324		\$1,862,693	\$2,891,891	\$0	\$1,862,693	\$0	\$0	\$0
E1.2	asphalt suburban pavement (Option B) (additional cost over chip seal)	222, 324		\$1,862,693	\$2,891,891	\$0	\$1,862,693	\$0	\$0	\$0
E1.3	Temporary Traffic Management	included		\$21,750	\$33,767	\$0	\$21,750	\$0	\$0	\$0
E1.4	Local Accommodation Works	included		\$320,850	\$498,120	\$0	\$320,850	\$0	\$0	\$0
F	Transport Initiatives			\$320,850	\$498,120	\$0	\$320,850	\$0	\$0	\$0
F5.0	Passenger transport initiative			\$300,000	\$465,750	\$0	\$300,000	\$0	\$0	\$0
F5.1b	New bus stops and bus shelters in the town centres			\$20,100	\$31,205	\$0	\$20,100	\$0	\$0	\$0
F5.1b	Temporary Traffic Management			\$750	\$1,164	\$0	\$750	\$0	\$0	\$0
G5.6	Local Accommodation Works - fences, crossings, etc			\$4,555,207	\$7,073,511	\$0	\$4,555,207	\$0	\$0	\$0
<b>Categories D, E &amp; F Total Cost</b>										
<b>Total</b>										
				Value Mgt	\$1,487,829	\$1,487,829	\$1,487,829	\$1,487,829	\$1,487,829	\$1,487,829
				100% Funding	\$10,717,736	\$10,717,736	\$10,717,736	\$10,717,736	\$10,717,736	\$10,717,736
				KQDC Cost	\$1,741,751	\$1,741,751	\$1,741,751	\$1,741,751	\$1,741,751	\$1,741,751
				By Others	\$466,914	\$466,914	\$466,914	\$466,914	\$466,914	\$466,914
				Total	\$18,025,325	\$18,025,325	\$18,025,325	\$18,025,325	\$18,025,325	\$18,025,325

Page 1 of 1







**Appendix Two:****Design Concepts – State highway 1 Reconfiguration**

The following reports were generated by the M2PP Alliance to explore and define design concepts for the future reconfiguration of State highway 1 between Poplar Ave Raumati and Peka Peka Road, Kapiti.:

***SH1 Reconfiguration (Kāpiti) Scheme Design Report*** dated 11 November 2011

- Volume 1 SH1 Reconfiguration (Kāpiti) Scheme Design Report
- Volume 2 Costing Schedules
- Volume 3 A3 size drawings
- Appendix 1: SH1 Revitalisation Traffic Assessment dated 30 May 2011  
(bound in Volume 1)
- Appendix 2: Current SH1 Modification Consultation Report dated October 2011  
(bound in Volume 1)

***SH1 (Kāpiti Coast) Revitalisation Options - Functionality Report*** dated 9 June 2011.

- Volume 1 SH1 (Kāpiti Coast) Revitalisation Options - Functionality Report
- Volume 2 A4 size appendices
- Volume 3 A3 size drawings

**DRAFT Report:*****Cycleway/Walkway Alternatives - SH1 Reconfiguration Project*** (13 June 2012)



Appendix Three:

State highway 1 Revocation - Three Step Process for Funding

1. The first step is to determine if the project is eligible for funding. This is done by checking the project against the criteria in the funding guidelines. If the project is eligible, the next step is to prepare a funding application.

2. The second step is to prepare a funding application. This involves completing the application form and providing the necessary supporting documents. The application should be submitted to the funding agency by the deadline.

3. The third step is to review the application.

a. The funding agency will review the application and decide if it is eligible for funding.

b. If the application is eligible, the funding agency will issue a funding decision letter.

c. The funding agency will also issue a funding agreement.

d. The funding agency will also issue a funding schedule of disbursements.

4. The fourth step is to implement the project. This involves carrying out the project in accordance with the funding agreement and the funding schedule of disbursements.

5. The fifth step is to monitor the project. This involves keeping track of the project's progress and ensuring that it is being carried out in accordance with the funding agreement.

6. The sixth step is to evaluate the project. This involves assessing the project's impact and determining if it has been successful.

7. The seventh step is to report on the project. This involves providing a report to the funding agency on the project's progress and impact.

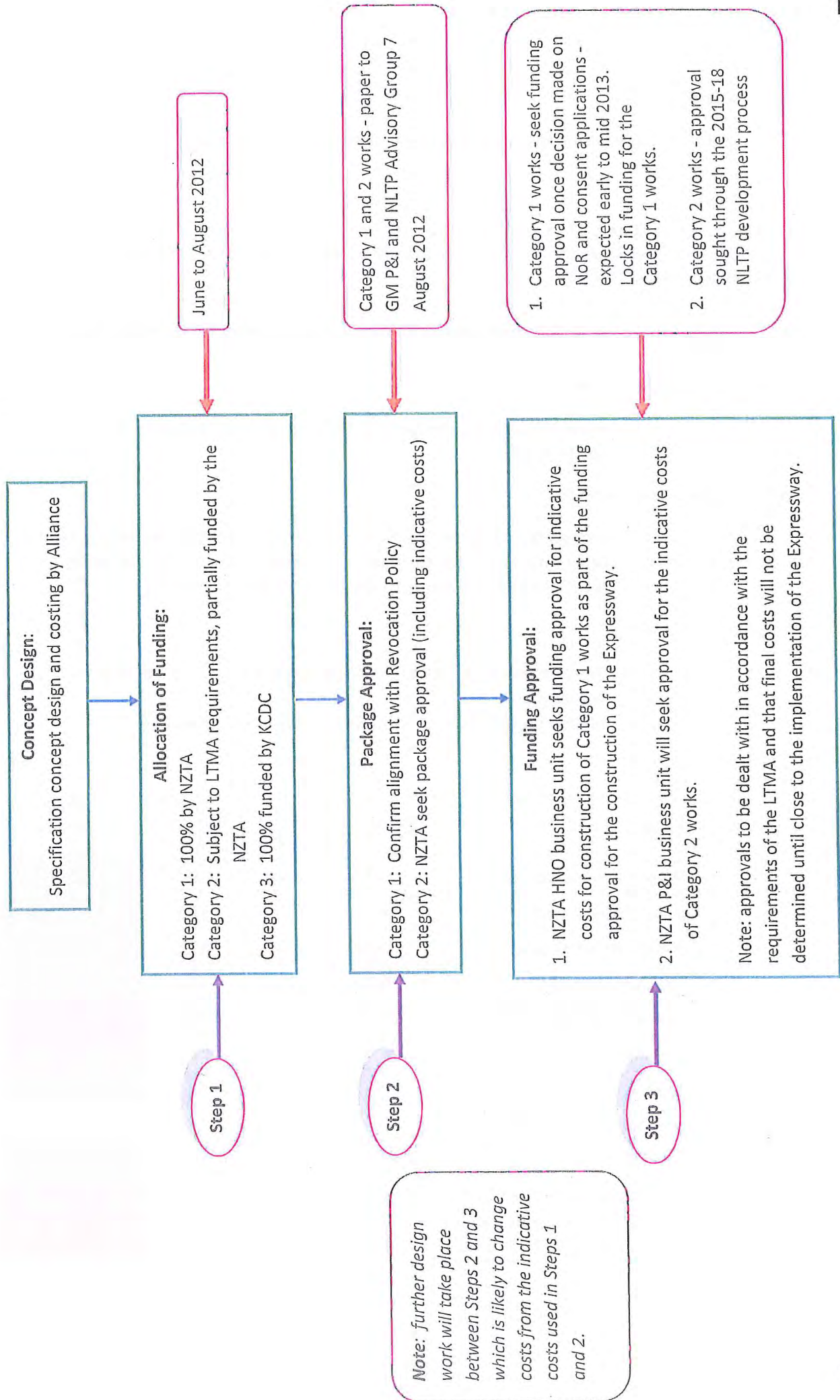
8. The eighth step is to close the project.

9. The ninth step is to evaluate the funding process. This involves assessing the funding process and determining if it has been successful.



## Appendix Three

### State Highway 1 Revocation: Three Step Process for Funding





**Appendix Four:****State Highway 1 (Kāpiti) Reconfiguration– Outline Specification****1 Scope**

This Outline Specification describes the standard of work scoped in the State Highway One (Kāpiti) Reconfiguration Scheme Design, 30th September 2011, completed by the M2PP Alliance for the NZTA.

**2 State Highway 1 Scheme Design Zones – Poplar Avenue to Peka Peka Road**

Town Entry Zones include:

- Poplar Avenue to Ihakara Street intersection, including the roundabouts

Town Centre Zones include:

- Paraparaumu Town Centre from Ihakara Street to Ventnor Drive
- Waikanae Town Centre from Waikanae Bridge to the Waikanae Northern Urban Limit (i.e. AMP's north boundary)

Rural Zones include:

- Ventnor Drive to Waikanae Bridge, excluding the Otaihanga Roundabout
- Waikanae Northern Urban Limit to Peka Peka Road

	Zone	CH Start	CH finish	Length/ m	Future Character Zone
1	Poplar to Raumati Rd	125	1400	1275	Town Entry
1a	Raumati new RBT	1400	1500	100	Town Entry
2	Raumati to Ihakara	1500	2070	570	Town Entry
2a	Ihakara new RBT	2070	2200	130	Town Entry
3	Ihakara to Kāpiti +120m	2200	3000	800	Town Centre
4	Amohia to Ventnor	3000	4300	1300	Town Centre
5	Ventnor to Otaihanga	4300	6300	2000	rural
-	Otaihanga RBT by others				
6	Otaihanga to Waikanae Bridge	6500	9350	2850	rural
7	Waikanae Bridge to Te Moana Rd	9350	9750	400	Town Centre
8	Te Moana Rd to Martin St	9750	10400	650	Town Centre



9	Martin to Waikanae Urban Limit	10400	11500	1100	Town Centre
10	Waikanae Urban Limit to Peka Peka	11500	13550	2050	rural
	Total Scheme Design length			13225	

**Table 1– State Highway 1 Zones table**

### **3 Carriageway Modifications**

#### **3.1 Services Relocations**

Service relocations shall be carried out in a manner as agreed with the service provider and located as to Councils approved layout.

#### **3.2 Trenching and Bedding.**

Standard bedding and trenching materials shall be used.

#### **3.3 Pipework**

Where pipe work needs relocating new pipes of similar size and standard will be installed. Note that small asbestos cement water mains are to be replaced with polyvinyl chloride (PVC) pipes and those 250mm diameter or larger are to be replaced with ductile iron (DI) pipes. Gravity asbestos cement wastewater pipes are to be replaced with PVC pipes and wastewater rising mains are to be replaced with polyethylene (PE) pipes. All pipework is to meet Council Standards and be approved by the Engineer.

#### **3.4 Stormwater Sumps**

Where stormwater sumps are to be relocated due to the kerb and channel moving, a similar sized sump is to be used. Where practicable the new sump shall line up with the existing sump lead to allow for extending or reducing the existing lead in a straight line. Standard Kāpiti Coast District Council trap sumps, or other Council approved trap sump arrangements, shall be used.

#### **3.5 Cable Relocations**

Where practicable pull the existing cables to one side to relocate. Where it is not practicable to pull existing cables to one side, a similar size and standard of cable shall be used.

#### **3.6 Pavement Removal and Stabilisation**

Where any pavement is to be removed it shall have saw cut edges and a kerb edging

#### **3.7 Carriageway Width**

For carriageway widths refer to State Highway 1 Reconfiguration (Kapiti) Scheme Design Drawings, 30 November 2011 (located in M2PP Library).

In summary:

- 3.5m wide traffic lanes
- 1.5m wide cycle lanes where the speed limit is less than 80 km/hr
- 2m wide cycle lanes where the speed limit is 80 km/hr or greater
- 0.5m wide rural shoulder



### **3.8 Topsoil and Grassing**

All grassed areas shall have a minimum of 150mm topsoil and the seed mixture shall be: 85% amenity ryegrass and 15% red fescue (or other as approved by the Council).

### **3.9 Pavement Reconstruction**

The following are guidelines only and may not be needed in all cases:

- Urban road reconstruction shall consist of geotextile (Terram 1500), 460mm sub-basecourse GAP65, 150 mm basecourse AP40 and 40mm AC (Mix 10).
- Where rural roads require widening for the cycle lanes the pavement shall be 200mm of AP65 sub-basecourse, 100mm of M4 basecourse and a two coat chipseal (with a subgrade CBR 4% minimum).

All materials used in pavement reconstruction shall comply with the relevant NZTA specifications.

## **4 Pavement Resurfacing**

### **4.1 Asphalt**

Roads in the urban areas with a speed environment of 50km/h will have an asphalt surface. Following the completion of road layout changes (e.g. shape corrections, median removal/addition and/or pavement reconstruction) a minimum of 20mm asphalt top coat will be applied.

If International Roughness Index is more than 150 then shape corrections shall be built up with up to 100mm of asphalt; for corrections beyond this a basecourse shall be required to build up the road level and 40mm of asphalt applied above this

### **4.2 Chip Seal**

Road surfaces beyond 50km/h areas shall be chip seal. The existing pavements shall be kept unchanged where possible. If road surfaces are required to be reshaped they shall be resealed with a two coat chipseal.

### **4.3 New Kerbs and Line Markings**

All raised medians, traffic islands etc shall have mountable nib kerbs and be infilled with the specified media (concrete, asphalt, blocks or topsoil and grass). All kerbs, channels and dish channels shall be a minimum 20MPa, 28-day strength and comply with NZS4404:2010.

### **4.4 Temporary Traffic Management**

Temporary Traffic Management shall comply with Part 8 of the NZTA Traffic control devices manual (TCD Manual), Dec 2008 (Code of Practice for Temporary Traffic Management (COPTTM) and any amendments).

## **5 Safety Works**

### **5.1 Intersection Modifications**

The new roundabouts shall have a raised, planted centre and concrete apron. The concrete apron shall have a minimum strength of 30MPa 28-day strength.



## **5.2 Traffic Signals**

All traffic signals hardware and software shall comply with the relevant current NZTA Standard Specifications and KCDC specifications.

## **5.3 Pedestrian crossings and safety measures**

Pedestrian crossings and tactile strips shall comply with the relevant current NZTA Standard Specifications.

## **5.4 Lighting**

All lighting shall comply with the relevant current NZTA Standard Specifications.

## **5.5 Signs**

All signs shall comply with the relevant current NZTA Standard Specifications.

## **5.6 Barriers**

Where barriers are to be removed (e.g. concrete or W section) they shall be sawn off and ground down flush with the ground level.

All barriers shall comply with the relevant current NZTA Standard Specifications.

## **5.7 Marker posts**

All marker posts shall comply with the relevant current NZTA Standard Specifications.

# **6 New Off Road Walkway/Cycle Ways**

## **6.1 Paving types**

### **Type One**

Shared foot/cycle paths from Poplar Avenue to Ihakara Street and Rural Areas.  
Compacted loose gravel – typically between 2m and 3m wide as drawn, and a minimum 150mm depth of compacted "Kapiti Blue" 40mm AP chip. Typically no kerb and channel. Edging shall be a minimum of 100 x 25 hardwood edging, staked and pinned with 50 x 50 x 400 posts at 750mm centres.

### **Type Two**

Suburban Areas - Asphalt path with concrete kerb and channel –typically 2.5m wide under Ausroads standards. . Minimum 25mm M10 asphaltic concrete on a maximum 100mm depth compacted 40mm basecourse (refer to figure 3.8 of NZS 4404:2004).

### **Type Three**

Urban Areas - Exposed aggregate in situ concrete path with concrete kerb and channel. Minimum 100mm compacted basecourse and 100mm thick concrete. Construction joints at 3m minimum centres unless steel reinforced. Concrete surfaces shall comply with NZS3114.

### **Type Four**

Special Areas - permeable paver units around trees - e.g. surrounding trees in raised pedestrian table in Paraparaumu town centre. Permeable pavers are to be approved by the Engineer.



## **6.2 Recycled Pavement Option for Rural Paths**

Reuse of redundant road pavement for rural walkways.

## **7 Bridge structures**

### **7.1 Wharemauku Stream Bridge**

The Wharemauku Stream Bridge is subject to specific design.

### **7.2 Rimutaka Rail Cycle Overbridge**

The Rimutaka Road rail over bridge shall be a minimum 2.5m wide, have a non skid deck surface and be located a minimum 3m away from the road bridge. The structure shall have a minimum single span of 38m and meet KiwiRail track clearance requirements. The Rimutaka Rail Cycle Overbridge is subject to detailed design but an indicative locational plan is set out in Appendix 5.

Refer to Transit SH1 Paraparaumu Rail Overbridge Cycle Bridge – Scheme Assessment Report, by Opus International Consultants, 14 January 2009.

### **7.3 Waikanae River Bridge**

Improvements will be made to the existing structure to increase cycling width (e.g. design of handrails) within budget parameters.

## **8 Retaining walls**

Low retaining walls to form walkways shall be either a proprietary interlocking concrete system or H4 treated timber proprietary system to Engineers approval.

## **9 Safety Barriers/Fences**

Safety barriers/fences shall be installed where the shared path is above a retaining wall or steep bank where there is a risk of falling. They shall comply with the New Zealand Building Code (NZBC).

## **10 Painting cycle lanes**

Painting new cyclelanes and road markings - paint finishes shall comply with the KCDC Standard Specifications.

## **11 Landscaping**

### **11.1 Services Relocations**

Refer to 3.1 above.

### **11.2 Pavement removal**

Refer also to 3.2.

Where pavement is to be removed to make way for tree planting, the full depth of pavement and basecourse is to be removed down to the subgrade.

Where existing road pavement is being removed to make way for grass and swale planting a minimum of 200mm depth of pavement should be removed, as approved by the Engineer.

### **11.3 Bioretention**



Bioretention swales and cells shall be designed in accordance with ARC TP10 as a minimum and have 100mm diameter heavy duty HDPE slotted underdrains with filter fabric socking. Where pavement is to be removed to make way for the bioretention devices the full depth of pavement and basecourse is to be removed down to the subgrade.

#### **11.4 Tree pits**

Trees in urban areas will be planted within 1.5m minimum diameter and 1.5m minimum depth concrete ring root containers set 200mm below the surface. Tree pits will be well drained and filled with a selected loam-sand mix plant media.

#### **11.5 Planting Types:**

The following indicative planting types have been identified:

#### **11.6 Mass Planting**

This includes a mix of native grasses and ground cover at close planting spacing. Species list includes: Tussock, Tauhinu, Renga Renga and Sand Coprosma (or other as approved by the Engineer)

#### **11.7 Mass Planting with tree enrichment**

As per Mass planting but with addition of tree species including: Kawakawa, Cabbage tree, Nikau, Titoki, Karaka and Kanuka (or other as approved by the Engineer)

#### **11.8 Trees**

Singular trees planted at even spacing with under planting groundcover.

#### **11.9 Clumps of trees**

Clumps / Groupings of trees with under planting. Tree species include: Cabbage Tree, Pohutukawa, Alder and Pin Oak (or other as approved by the Engineer).

#### **11.10 Swales**

Swale planting includes (but not limited to): Carax, Baumea, and Coraderia. Areas that are identified for swales may require further design to calculate the feasibility and function of the swale. Species of swale planting may be selected even if the swale does not function as a low impact design bioremediation swale.

#### **11.11 Urban Trees**

Singular Tree generally planted in large tree pits with a permeable surface treatment. Trees include (but not limited to): Poplar, Eucalyptus, Pin Oak, Alder and Pohutukawa.

#### **11.12 Grass**

Mown grass generally occurs in suburban and rural character zones / sections.

All grassed areas shall have a minimum of 150mm topsoil and the seed mixture shall be: 85% amenity ryegrass and 15% red fescue (or other as approved by the Council).

#### **11.13 Street Furniture**

Street furniture shall be provided by Council to its specification.

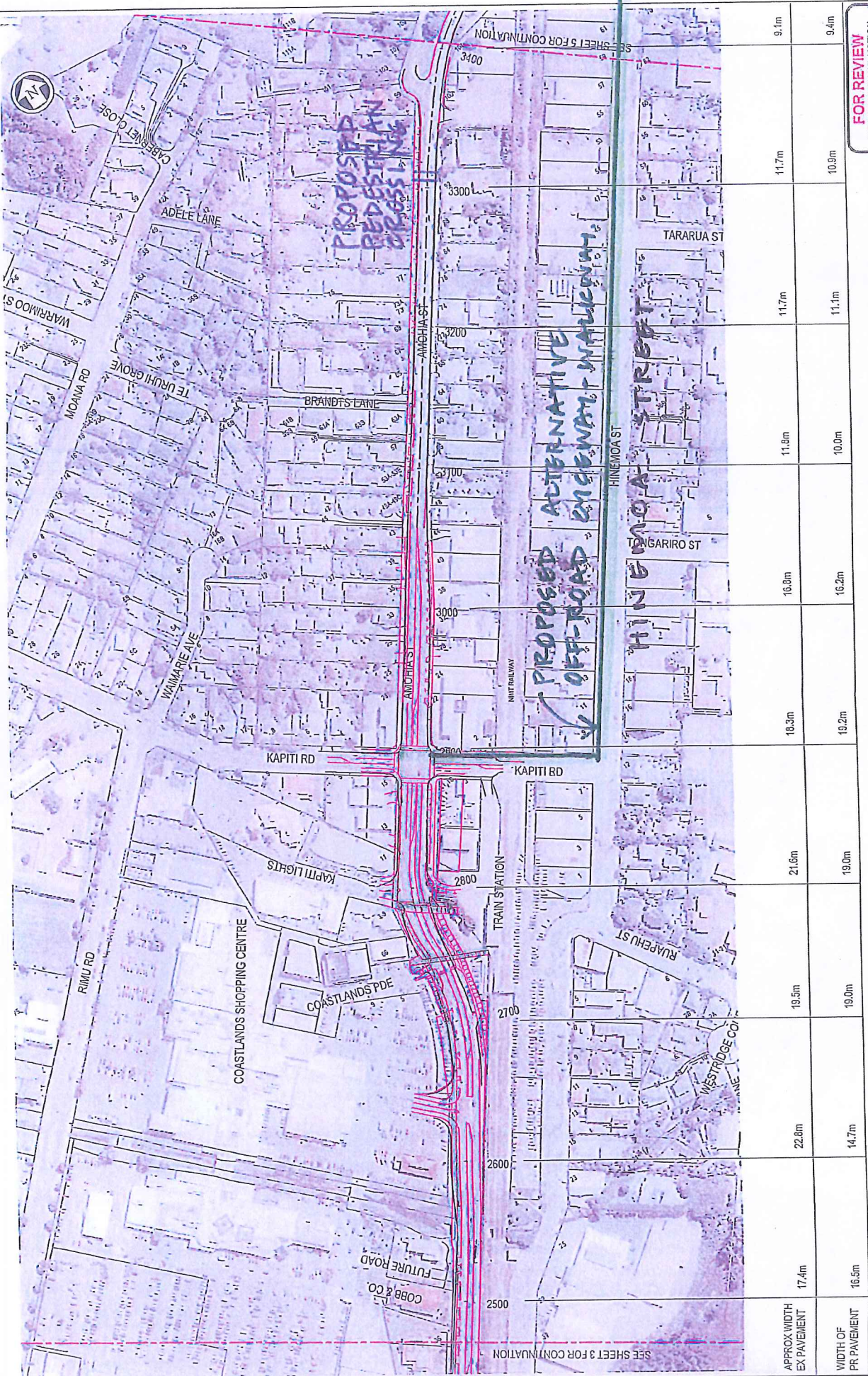


## Appendix Five:

### Indicative Plans of Rimutaka Rail Cycle Overbridge



Drawn: P26/04 25/09/2011 10:24 am



FOR REVIEW  
NOT FOR CONSTRUCTION

SH-SP-004  
A

PROPOSED ROAD LAYOUT  
SHEET 4 OF 16

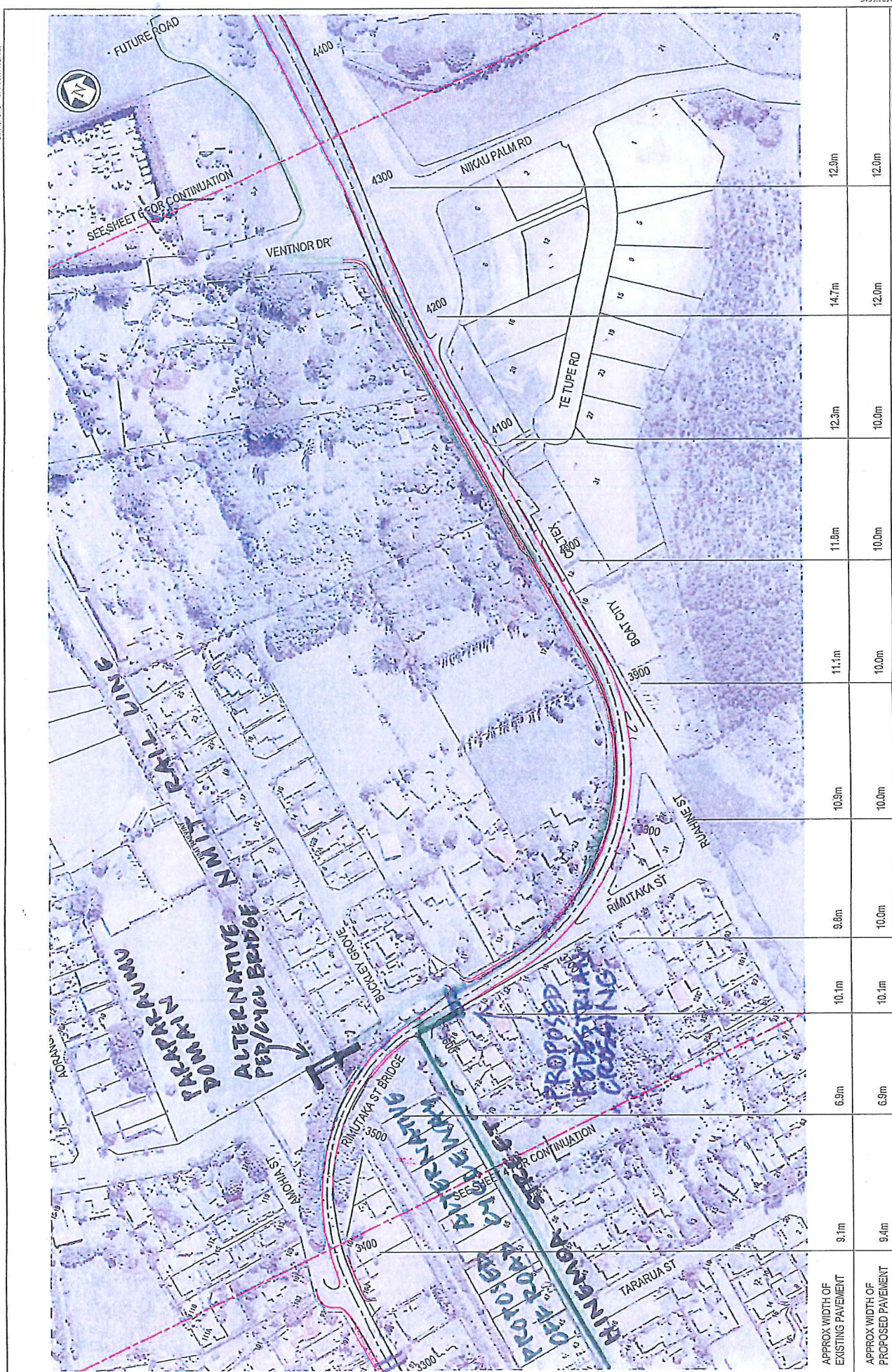
SH1 RECONFIGURATION  
POPULAR AVENUE TO PEKA PEKA ROAD  
RP 10120.00 TO 10233.61

MAKAKAYS TO PEKA PEKA  
NEW ZEALAND  
TRANSPORT  
AGENCY

Document created by: NZTA by JSP/PP/Alaines, Level 2, 17-21 Vintners St, WELLINGTON

DATE	BY	REVISION	DESCRIPTION
25/09/2011	P26/04	1	ISSUED FOR REVIEW
25/09/2011	P26/04	2	ISSUED FOR REVIEW
25/09/2011	P26/04	3	ISSUED FOR REVIEW
25/09/2011	P26/04	4	ISSUED FOR REVIEW
25/09/2011	P26/04	5	ISSUED FOR REVIEW
25/09/2011	P26/04	6	ISSUED FOR REVIEW
25/09/2011	P26/04	7	ISSUED FOR REVIEW
25/09/2011	P26/04	8	ISSUED FOR REVIEW
25/09/2011	P26/04	9	ISSUED FOR REVIEW
25/09/2011	P26/04	10	ISSUED FOR REVIEW





FOR REVIEW  
NOT FOR CONSTRUCTION

CLOSED ROAD LAYOUT  
SHEET 5 OF 16

SH1 RECONFIGURATION  
POPLAR AVENUE TO PEKA PEKA ROAD  
RP 1012/0.00 TO 1023/3.61

**Mackays to Peka Peka**



NZ TRANSPORT  
AGENCY

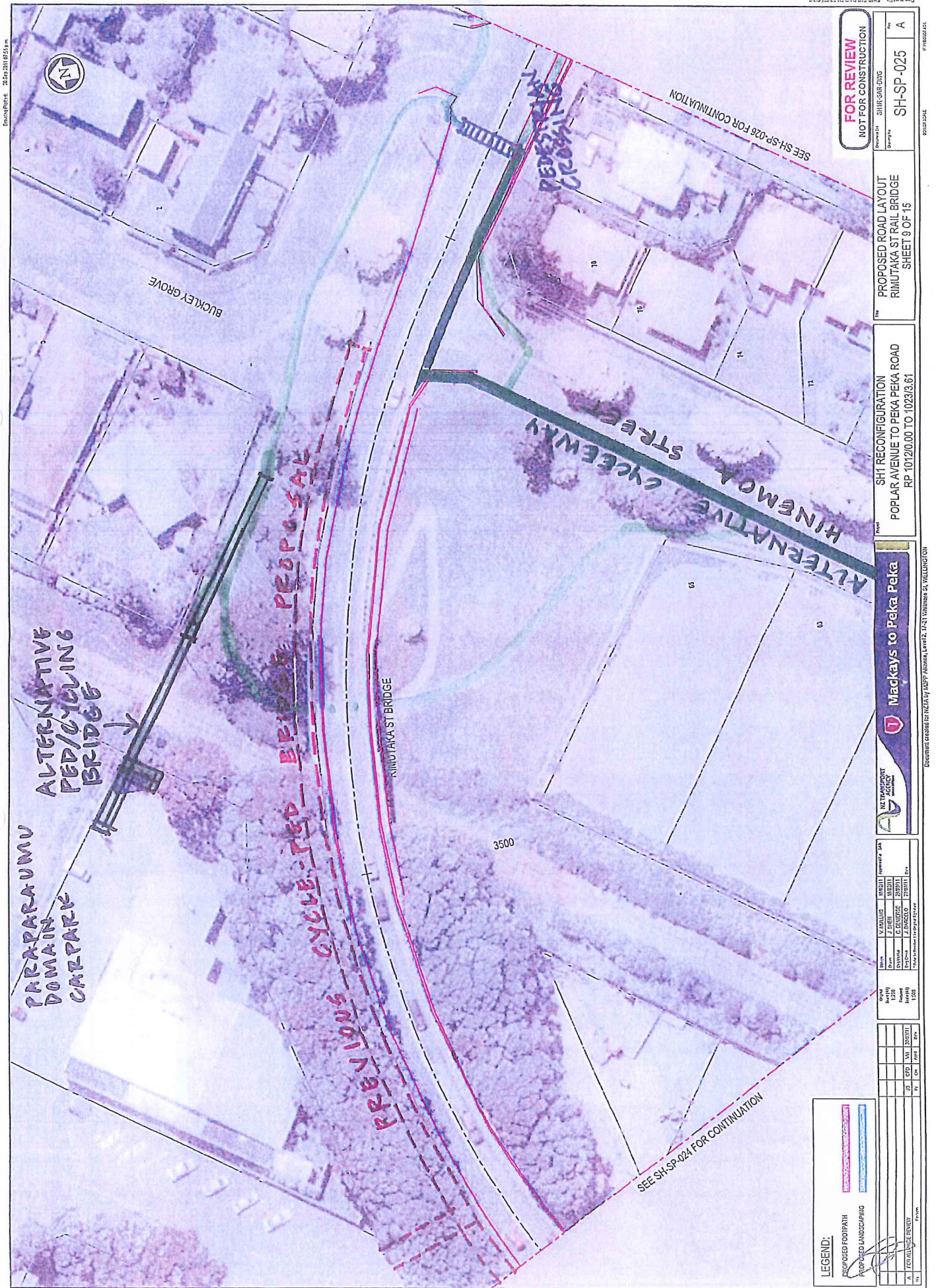
NAME		DATE	TIME
J. BARCELLO	11/06/81		
C. DEVEREUX	11/07/81		
K. HIGGINS	11/08/81		
V. MARRAS	11/09/81		

[illegible]

FOR ALLIANCE REVIEW

2. 17-21 Vihulmota St, WELLINGTON





**LEGEND:**

PROPOSED FOOTPATH	PROPOSED LANDSCAPING
EXISTING FOOTPATH	EXISTING LANDSCAPING
EXISTING ROAD	EXISTING CYCLEWAY
EXISTING RAIL	EXISTING BRIDGE

**FOR REVIEW**  
NOT FOR CONSTRUCTION

Project: SH-SP-025  
A

PROPOSED ROAD LAYOUT  
RIMUTAKA ST RAIL BRIDGE  
SHEET 9 OF 15

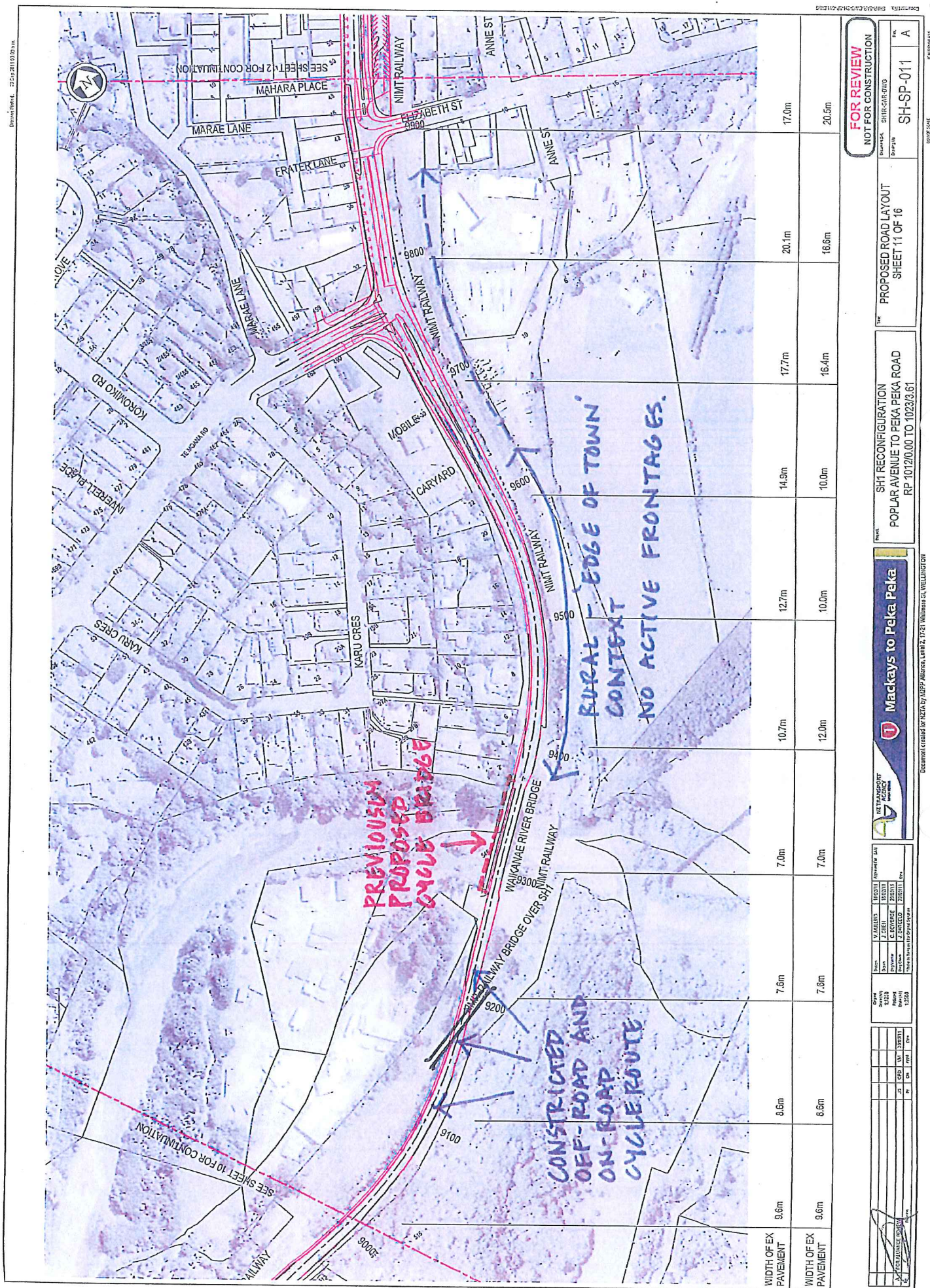
SH1 RECONFIGURATION  
POPLAR AVENUE TO PEKA PEKA ROAD  
RP 101200.00 TO 10233.61

**1** Mackays to Peka Peka  
ALTRANSPORT AGENCY

Drawn	Checked	Designed	Reviewed	Approved
J. ELLIOTT	C. ELLIOTT	C. ELLIOTT	C. ELLIOTT	C. ELLIOTT
12/01/2011	12/01/2011	12/01/2011	12/01/2011	12/01/2011

Scale	North Arrow	Project Name	Sheet Number
1:500	True	SH-SP-025	9 OF 15







**MacKays to Peka Peka Expressway**

Level 2, 17-21 Whitmore Street  
PO Box 8044, Wellington 6140, New Zealand  
T: 0508 M2PP INFO (0508 6277 4636)  
E: info@m2pp.co.nz

Pat Dougherty  
Chief Executive  
Kāpiti Coast District Council

7 August 2012

Dear Pat,

I am writing with reference to the Project Agreement with respect to Kāpiti Expressway: Process for Revocation of State Highway 1, signed by you, Lyndon Hammond (NZTA, Planning and Investments) and me, 7 August 2012.

The SH1 Kāpiti Reconfiguration Project Costing Matrix Revision 18 (attached to this letter) included as Appendix One to the Agreement, shows a total indicative costing of \$18,025,325. This differs from the latest Revision 19 (attached to this letter) which shows a total indicative costing of \$21,301,100.

The cost items included in Revision 19 but not in Revision 18 relate to temporary traffic management, and, as previously agreed, I can confirm that NZTA accepts full responsibility for these.

I am of the opinion that NZTA will be able to satisfy the requirements through efficient management of our own planned maintenance traffic management works, and within our existing operational budgets. If this is not achieved, then any additional cost for these activities will be covered by the budget of the NZTA, State Highway Manager, Wellington.

In effect, this letter guarantees that the cost items included in Revision 19 which were not included in Revision 18 will be covered by NZTA.

Yours sincerely

Rod James  
State Highway Manager, Wellington





## MacKays to Peka Peka Expressway

Level 2, 17-21 Whitmore Street  
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E: [info@m2pp.co.nz](mailto:info@m2pp.co.nz)

Attached: Revisions 18 and 19 of the SH1 Kāpiti Reconfiguration Project Costing Matrix



31 July 2012

# Revision 18

## INDICATIVE COSTINGS

31 July 2012									
Priority Area: Waikanae Town Centre (including zones 7, 8 & 9)									
NZTA Work Category	Guiding Objectives For PAB	Net Cost	Gross Cost	Value Management Reduction	100% NZTA Funding	KDC 47% Cost	Pending Unallocated	Funded By Others	
A1.0	Carriageway Modification	\$2,906,655	\$4,514,582		\$2,212,388	\$324,605	\$714,885	\$0	
A1.1	services relocation	\$874,680	\$1,357,341	\$0	\$1,209,183	\$0	\$0	\$0	
A1.2	pavement removal	\$39,657	\$52,618		\$332,982				
A1.3	pavement reconstruction/new pavement	\$214,461	\$322,982		\$32,618				
A1.4	re-surface road pavement	\$146,652	\$227,678		\$227,678				
A1.5	new kerbs and line marking	\$57,652	\$89,595		\$89,595				
A3.1	Raised Median - Top soil	\$127,248	\$197,553		\$197,553				
A3.2	Raised Median - Grassing		\$0		\$0				
A3.3	Top soil	\$159,085	\$246,979		\$246,979				
A3.4	Grassing	\$10,518	\$16,329		\$16,329				
A3.5	Temporary Traffic Management	\$95,012	\$149,717						
A3.6	Local Accommodation Works	\$3,575	\$5,550		\$5,550				
A3.7	Safety Improvement	\$995,888	\$1,546,116	\$0	\$1,003,205	\$51,083	\$0	\$0	
A3.8	Intersection modifications	\$350,000	\$543,375		\$543,375				
A3.9	Signs and lighting	\$187,400	\$280,939		\$290,939				
A3.10	pedestrian crossings	\$100,000	\$155,250		\$155,250				
A3.11	Temporary Traffic Management	\$255,452	\$395,566						
A3.12	Local Accommodation Works	\$8,787	\$13,641		\$13,641				
A3.13	New off road cycleway	\$82,574	\$128,197		\$128,197				
A3.14	New walkway/cycleway bridge at Waikanae Bridge	\$0	\$0		\$0				
A3.15	Temporary Traffic Management	\$30,506	\$47,361						
A3.16	Local Accommodation Works	\$1,138	\$1,767		\$1,767				
A3.17	Landscaping	\$440,474	\$683,835	\$0	\$0	\$381	\$0	\$683,835	\$0
A3.18	Ground cover, mass planting and specimen trees	\$368,880	\$572,841						
A3.19	services relocation	\$71,494	\$110,994		\$0	\$297,163	\$263,522	\$110,994	\$0
A4.0	Main Street Upgrade	\$381,150	\$591,735		\$0	\$297,163	\$263,522	\$31,050	\$0
A4.1	New walkways in town centres	\$361,150	\$560,685						
A4.2	Street furniture	\$214,464	\$332,955		\$0	\$0	\$0	\$31,050	\$0
A4.3	Sustainability initiative	\$214,464	\$332,955		\$0	\$0	\$0	\$0	\$0
A7.0	Bio-retention swales and devices	\$3,810,285	\$5,915,468	\$-332,955	\$2,024,132	\$336,532	\$1,295,489	\$0	\$0
B1.0	Carriageway Modification	\$1,100,400	\$1,708,372	\$-41,531	\$1,501,186	\$0	\$0	\$0	\$0
B1.1	services relocation	\$464,524	\$721,173		\$721,173				
B1.2	pavement reconstruction/new pavement	\$77,522	\$120,353		\$120,353				
B1.3	re-surface road pavement	\$71,439	\$110,009		\$110,009				
B1.4	new kerbs and line marking	\$28,084	\$43,600		\$43,600				
B1.5	Raised Median - Top soil	\$142,678	\$221,508		\$221,508				
B3.1	Raised Median - Grassing	\$0	\$0		\$0				
B3.2	Top soil	\$166,708	\$258,814		\$258,814				
B3.3	Grassing	\$11,022	\$17,112		\$17,112				
B3.4	Temporary Traffic Management	\$13,115	\$20,714						
B3.5	Local Accommodation Works	\$4,979	\$7,730		\$7,730				
B3.6	Safety Improvement	\$1,328,868	\$2,063,067	\$0	\$522,934	\$557,237	\$0	\$0	\$0
B3.7	Intersection modifications	\$0	\$0		\$0				
B3.8	Signs and lighting	\$135,000	\$209,588		\$209,588				
B3.9	pedestrian crossings	\$100,000	\$155,250		\$155,250				
B3.10	Temporary Traffic Management	\$49,133	\$76,294						
B3.11	Local Accommodation Works	\$1,804	\$2,847		\$2,847				
B3.12	Wharemauku Stream Bridge	\$111,992	\$173,668		\$173,668				
B3.13	Pedestrian crossing @ Anohia St	\$100,000	\$155,250		\$155,250				
B3.14	New walkway/cycleway bridge near Rimuaka St Rail Bridge	\$645,000	\$1,001,353		\$0	\$470,640			
B3.15	Temporary Traffic Management	\$17,813	\$27,927						
B3.16	Local Accommodation Works	\$6,687	\$10,382						
B3.17	Landscaping	\$814,453	\$1,284,439	\$0	\$0	\$4,879	\$1,284,439	\$0	\$0
B3.18	Ground cover, mass planting and specimen trees	\$659,612	\$1,024,048						
B3.19	services relocation	\$154,841	\$240,391		\$0	\$1,024,048			
B4.0	Main Street Upgrade	\$359,813	\$558,060	\$0	\$0	\$427,715	\$376,284	\$31,050	\$0
B4.1	New walkways in town centres	\$430,250	\$662,438			\$345,792	\$306,646		
B4.2	Street furniture	\$20,000	\$31,050						
B4.3	Pedestrian Table in Paraparaumu town centre	\$89,595	\$151,571		\$0	\$81,933	\$72,649	\$31,050	\$0
B7.0	Sustainability initiative	\$26,751	\$41,531	\$-41,531	\$0				
B7.1	Bio-retention swales and devices	\$26,751	\$41,531	\$-41,531	\$0				
C1.0	Carriageway Modification	\$2,323,271	\$4,538,378	\$-219,882	\$2,587,595	\$76,239	\$580,122	\$0	\$0
C1.1	services relocation	\$1,323,651	\$2,054,968	\$0	\$1,889,477	\$0	\$0	\$0	\$0
C1.2	pavement removal	\$338,475	\$500,732		\$500,732				
C1.3	pavement reconstruction/new pavement	\$211,460	\$321,160		\$321,160				
C1.4	re-surface road pavement	\$66,449	\$103,162		\$103,162				
C1.5	new kerbs and line marking	\$0	\$0		\$0				
C3.1	Raised Median - Top soil	\$530,459	\$828,195		\$828,195				
C3.2	Raised Median - Grassing	\$38,488	\$59,753		\$59,753				
C3.3	Top soil	\$106,597	\$165,491						
C3.4	Grassing	\$3,977	\$6,175		\$6,175				
C3.5	Temporary Traffic Management	\$3,977	\$6,175						
C3.6	Local Accommodation Works	\$23,977	\$36,175		\$36,175				
C3.7	Safety Improvement	\$164,406	\$255,240	\$0	\$688,118	\$0	\$0	\$0	\$0
C3.8	Raumati Intersection modification	\$313,995	\$482,227		\$255,240				
C3.9	hakara Intersection modification	\$68,500	\$106,346		\$322,227				
C3.10	Signs and lighting	\$0	\$0		\$106,346				
C3.11	pedestrian crossings	\$71,101	\$115,356		\$0				
C3.12	Temporary Traffic Management	\$2,773	\$4,304		\$4,304				
C3.13	Local Accommodation Works	\$2,773	\$4,304		\$0	\$85,972	\$76,239	\$0	\$0
C3.14	Walkway/Cycleway Initiative	\$104,484	\$162,211		\$0	\$85,972	\$76,239	\$0	\$0
C3.15	New off road cycleway	\$507,647	\$788,122		\$0	\$0	\$788,122	\$0	\$0
C3.16	Landscaping	\$507,647	\$788,122		\$0	\$0	\$788,122	\$0	\$0
C3.17	Ground cover, mass planting and specimen trees	\$403,512	\$619,602		\$0	\$0	\$788,122	\$0	\$0
C7.0	Sustainability initiative	\$403,512	\$619,602		\$0	\$0	\$788,122	\$0	\$0
C7.1	Bio-retention swales and devices	\$403,512	\$619,602		\$0	\$0	\$788,122	\$0	\$0
Priority Areas A, B & C Total Cost									
D	Rural zones (zones 5, 6 and 10)	\$1,524,862	\$2,367,348		\$590,520	\$404,375	\$136,325	\$0	\$0
D1.0	Carriageway Modification	\$760,374	\$1,180,480		\$520,658	\$0	\$0	\$0	\$0
D1.1	services relocation	\$112,680	\$174,936		\$174,936				
D1.2	pavement removal	\$25,438	\$41,044		\$41,044				
D1.3	pavement reconstruction/new pavement	\$0	\$0		\$0				
D1.4	re-surface road pavement	\$53,104	\$82,019		\$129,019				
D1.5	new kerbs and line marking	\$86,552	\$137,461		\$129,019				
D3.1	Top soil	\$35,854	\$59,088		\$137,461				
D3.2	Grassing	\$16,750	\$26,109		\$9,088				
D3.3	Temporary Traffic Management	\$16,750	\$26,109		\$29,109				
D3.4	Local Accommodation Works - fences, crossings, etc	\$45,000	\$69,853		\$69,853				
D3.5	Safety Improvement	\$45,000	\$69,853		\$69,853				
D3.6	Intersection modifications	\$341	\$0		\$0				
D3.7	Signs and lighting	122	\$0		\$0				
D3.8	pedestrian crossings	122,451	\$0		\$0				
D3.9	Walkway/Cycleway Initiative	4.9b, 10c	\$531,679	\$850,681	\$0	\$455,997	\$404,375	\$0	\$0
D3.10	New off road cycleway	124, 452	\$516,653	\$802,060		\$455,997	\$376,968	\$0	\$0
D3.11	Temporary Traffic Management	included	\$77,294	\$120,300		\$455,997	\$376,968	\$0	\$0
D3.12	services relocation	213, 231	\$37,260	\$58,312		\$455,997	\$376,968	\$0	\$0
D3.13	Landscaping	10c	\$87,610	\$136,325		\$455,997	\$376,968	\$0	\$0
D3.14	Ground cover, mass planting and specimen trees		\$87,610	\$136,325		\$455,997	\$376,968	\$0	\$0
D3.15	(additional cost over grass)		\$87,610	\$136,325		\$455,997	\$376,968	\$0	\$0
D7.0	Sustainability initiative	11a	\$2,710,495	\$4,208,043		\$0	\$0	\$0	\$0
D7.1	Bio-retention swales and devices	121	\$2,710,495	\$4,208,043		\$0	\$0	\$0	\$0
E1.0	Carriageway Modification								



# Revision 19

## INDICATIVE COSTINGS

Priority Areas: Wairarapa Town Centre (including zones 7, 8 & 9)		NZTA Work Category	Guiding Objectives For PAB	Net Cost	Gross Cost	Value Management Reduction	100% Funding	KDC-45% Cost	Funded By Others
A									
A1.0	Carriageway Modification	213231	3d, 4, 9c, 10c	\$3,006,655	\$4,657,432	\$0	\$2,772,429	\$449,200	\$0
A1.1	services relocation	111		\$974,680	\$1,357,941		\$1,357,941	\$0	\$0
A1.2	pavement removal	111		\$39,657	\$33,982		\$33,982	\$0	\$0
A1.3	pavement reconstruction (new pavement)	111		\$146,652	\$227,678		\$227,678	\$0	\$0
A1.4	re-surface road pavement	111, 213, 214		\$89,505	\$197,553		\$197,553	\$0	\$0
A1.5	new kerbs and line marking	214		\$0	\$0		\$0	\$0	\$0
A3.1	Raised Median - Top soil	214		\$159,085	\$245,970		\$245,970	\$0	\$0
A3.2	Raised Median - Grassing	214		\$10,518	\$16,329		\$16,329	\$0	\$0
A3.3	Grassing	214		\$5,912	\$10,777		\$10,777	\$0	\$0
G1.1	Temporary Traffic Management	included		\$3,573	\$5,530		\$5,530	\$0	\$0
G2.1	Local Accommodation Works	included		\$1,095,888	\$1,701,366		\$1,701,366	\$0	\$0
A2.0	Safety Improvement	341	3d, 4, 9c, 10c	\$350,000	\$543,375		\$543,375	\$0	\$0
A3.1	Intersection modifications	122		\$17,400	\$20,930		\$20,930	\$0	\$0
A3.2	Signs and lighting	122, 451		\$100,000	\$155,250		\$155,250	\$0	\$0
A3.3	pedestrian crossings	included		\$23,000	\$23,000		\$23,000	\$0	\$0
G1.1	Temporary Traffic Management	included		\$7,715	\$11,978		\$11,978	\$0	\$0
G2.1	Local Accommodation Works	included		\$82,574	\$128,107		\$128,107	\$0	\$0
A2.4	New off road cycleway	451, 452		\$100,000	\$155,250		\$155,250	\$0	\$0
A2.5b	New off road cycleway bridge at Wairarapa Bridge	included		\$2,210	\$3,451		\$3,451	\$0	\$0
G1.1	Temporary Traffic Management	included		\$2,210	\$3,451		\$3,451	\$0	\$0
G2.1	Local Accommodation Works	included		\$400,474	\$689,835		\$689,835	\$0	\$0
A3.0	Landscaping			\$388,880	\$572,841		\$572,841	\$0	\$0
A3.2a	Ground cover, mass planting and specimen trees (additional cost over grass)			\$71,484	\$110,994		\$110,994	\$0	\$0
A1.1	services relocation	213231	9b	\$381,150	\$599,735		\$599,735	\$0	\$0
A4.0	Main Street Upgrade	451		\$381,150	\$599,735		\$599,735	\$0	\$0
A4.1	New walkways in town centres			\$20,000	\$33,955		\$33,955	\$0	\$0
A4.2	Street furniture			\$214,464	\$332,955		\$332,955	\$0	\$0
A7.0	Sustainability Initiative	121		\$2,810,265	\$5,915,465		\$5,915,465	\$0	\$0
A7.1	Bio-retention swales and devices			\$1,100,400	\$1,708,372		\$1,708,372	\$0	\$0
B	Priority Area: Paraparaumu Town Centre (including zones 3 & 4)								
B1.0	Carriageway Modification	213231	4, 9b, 9c, 10c	\$464,324	\$721,173		\$721,173	\$0	\$0
B1.1	services relocation	111		\$77,522	\$124,353		\$124,353	\$0	\$0
B1.2	pavement removal	111		\$71,459	\$110,800		\$110,800	\$0	\$0
B1.3	pavement reconstruction (new pavement)	111, 213, 214		\$28,064	\$43,600		\$43,600	\$0	\$0
B1.4	re-surface road pavement	214		\$142,978	\$221,506		\$221,506	\$0	\$0
B1.5	new kerbs and line marking	214		\$0	\$0		\$0	\$0	\$0
B3.1	Raised Median - Top soil	214		\$166,708	\$255,014		\$255,014	\$0	\$0
B3.2	Raised Median - Grassing	214		\$11,022	\$17,712		\$17,712	\$0	\$0
B3.3	Top soil	214		\$1,079	\$1,714		\$1,714	\$0	\$0
B3.4	Grassing	214		\$1,679	\$2,730		\$2,730	\$0	\$0
G1.1	Temporary Traffic Management	included		\$1,329,868	\$2,063,067		\$2,063,067	\$0	\$0
G2.2	Local Accommodation Works	included		\$155,000	\$239,588		\$239,588	\$0	\$0
B2.0	Safety Improvement	122		\$100,000	\$155,250		\$155,250	\$0	\$0
B2.1	Intersection modifications	122		\$100,000	\$155,250		\$155,250	\$0	\$0
B2.2	Signs and lighting	122, 451		\$100,000	\$155,250		\$155,250	\$0	\$0
B2.3	pedestrian crossings	included		\$2,847	\$4,417		\$4,417	\$0	\$0
G2.2	Local Accommodation Works	included		\$111,952	\$173,568		\$173,568	\$0	\$0
B2.4	Whareroa Street Bridge	451, 452		\$100,000	\$155,250		\$155,250	\$0	\$0
D2.5a	Pedestrian crossing at Arorua St	451		\$645,000	\$1,001,363		\$1,001,363	\$0	\$0
B2.5b	New off road cycleway bridge near Rurumaka St Rail Bridge	451, 452		\$170,217	\$271,331		\$271,331	\$0	\$0
G1.1	Temporary Traffic Management	included		\$6,687	\$10,382		\$10,382	\$0	\$0
G2.2	Local Accommodation Works	included		\$814,453	\$1,264,439		\$1,264,439	\$0	\$0
B3.0	Landscaping			\$659,612	\$1,024,048		\$1,024,048	\$0	\$0
B3.2a	Ground cover, mass planting and specimen trees (additional cost over grass)			\$154,841	\$240,391		\$240,391	\$0	\$0
B1.1	services relocation	213231	9b	\$539,813	\$838,060		\$838,060	\$0	\$0
B4.0	Main Street Upgrade	451		\$420,250	\$652,438		\$652,438	\$0	\$0
B4.2	Street furniture			\$20,000	\$33,955		\$33,955	\$0	\$0
B1.3	Pedestrian Traffic Management			\$99,563	\$154,371		\$154,371	\$0	\$0
B7.0	Sustainability Initiative	121		\$2,810,265	\$5,915,465		\$5,915,465	\$0	\$0
B7.1	Bio-retention swales and devices			\$1,100,400	\$1,708,372		\$1,708,372	\$0	\$0
C	Priority Area: Paparua to Ihakara (including town entry zones 1-2b)								
C1.0	Carriageway Modification	213231	4, 9b, 9c	\$436,475	\$680,732		\$680,732	\$0	\$0
C1.1	services relocation	111		\$136,200	\$211,480		\$211,480	\$0	\$0
C1.2	pavement removal	111		\$66,440	\$103,162		\$103,162	\$0	\$0
C1.3	pavement reconstruction (new pavement)	111, 213, 214		\$0	\$0		\$0	\$0	\$0
C1.4	re-surface road pavement	214		\$523,450	\$808,105		\$808,105	\$0	\$0
C1.5	new kerbs and line marking	214		\$308,480	\$480,753		\$480,753	\$0	\$0
C3.1	Raised Median - Top soil	214		\$106,007	\$165,461		\$165,461	\$0	\$0
C3.2	Raised Median - Grassing	214		\$3,077	\$5,175		\$5,175	\$0	\$0
C3.3	Top soil	214		\$523,977	\$813,474		\$813,474	\$0	\$0
C3.4	Grassing	214		\$164,406	\$255,240		\$255,240	\$0	\$0
G1.1	Temporary Traffic Management	included		\$213,995	\$332,237		\$332,237	\$0	\$0
G2.2	Local Accommodation Works	included		\$30,500	\$47,750		\$47,750	\$0	\$0
G2.3	Local Accommodation Works	included		\$4,000	\$6,312		\$6,312	\$0	\$0
C2.4	New off road cycleway	121, 452	4, 9c, 10c	\$104,484	\$162,211		\$162,211	\$0	\$0
C3.0	Landscaping			\$507,647	\$788,122		\$788,122	\$0	\$0
C3.2a	Ground cover, mass planting and specimen trees (additional cost over grass)			\$463,512	\$719,602		\$719,602	\$0	\$0
C7.0	Sustainability Initiative	121		\$653,512	\$1,001,363		\$1,001,363	\$0	\$0
C7.1	Bio-retention swales and devices			\$1,100,400	\$1,708,372		\$1,708,372	\$0	\$0
Priority Areas A, B & C Total Cost				\$9,740,211	\$16,121,678		\$16,121,678	\$0	\$0
Priority Areas D, E & F Total Cost				\$4,556,207	\$7,073,611		\$7,073,611	\$0	\$0
Total				\$14,296,418	\$22,195,109		\$22,195,109	\$0	\$0