

OFFICER'S REPORT FOR: Hearing Commissioner

IN THE MATTER OF: An application for Resource consent for to construct and operate a retail activity and earthworks exceeding the permitted activity standards at 160 Kāpiti Road, Paraparaumu

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REPORT DATED: 1 March 2022

DATE OF HEARING: 22 March 2022

Publicly Notified Application

Site Address	160 Kāpiti Road, Paraparaumu
Legal Description	Lots 1 & 2 DP 63027 and Lot 3 DP 63992
Applicant	Kāpiti Retail Holdings Limited
Proposal	To undertake earthworks that do not meet the permitted activity standards for the construction of a building to be occupied by a retail activity that does not meet the permitted activity standards for the General Industrial Zone, with regards to; vehicle movements, signage, access, and landscaping and requires consent under the National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health
District Plan Notations	Zoned – General Industrial Land subject to Designation 301 (Plantation Reserve) for the Kāpiti Coast District Council Ponding flood hazard Coastal Environment
Owner	Ballinger Industries Limited
Activity Status	Non-Complying

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APPENDICES

- A Receiving Environment Resource Consents
- B Submissions
- C Statement of Evidence of Neil Trotter
- D Recommended Conditions

Statement of Experience - Marnie Rydon

- 1) My name is Marnie Rydon. I hold a Bachelor of Social Sciences from the University of Waikato. I am a member of the Resource Management Law Association.
- 2) I have over nine years' experience in resource management in New Zealand. This includes extensive experience in local authorities.
- 3) My current position is as a Resource Management Consultant at Incite, a resource management and environmental consultancy based in Wellington.
- 4) I am very familiar with the planning framework on the Kāpiti Coast as prior to my current position I worked for the Kāpiti Coast District Council for six years.
- 5) I have read and am familiar with the Code of Conduct for Expert Witnesses in the current Environment Court Practice Note (2014), have complied with it, and will follow the Code when presenting evidence. I also confirm that the matters addressed are within my area of expertise, except where relying on the opinion or evidence of other witnesses. I have not omitted to consider material facts known to me that might alter or detract from the opinions expressed.

1. INTRODUCTION

- 6) This report discusses and makes recommendations on submissions received in relation to an application by Kāpiti Retail Holdings Limited for resource consent to construct and operate a supermarket located at 160 Kāpiti Road, Paraparaumu.
- 7) Specifically, the purpose of application is to:
 - Construct and operate a 7-day a week retail activity (supermarket) with associated offices, signage and landscaping within the General Industrial Zone;
 - Earthworks to facilitate the construction of the supermarket to construct a building platform above the 1% AEP;
 - Exceed the maximum permitted vehicle movements per day;
 - Change the use and undertake earthworks on a site where known Hazardous Activities and Industries List activities have been undertaken.

2. RELEVANT BACKGROUND

2.1. Site History

- 8) The location of the proposed supermarket was previously home to PlaceMakers trade store and timber yard. This store relocated to another site on Kāpiti Road in 2017. Since this time, the building has remained empty with temporary occupation for a Christmas experience and various outdoor storage uses.
- 9) The wider site contains a motor vehicle sales yard (Capital City Ford) and trade suppliers (Carpet Court) who will continue to occupy portions of the site.
- 10) The site is legally described as Lots 1 & 2 DP 63027 and Lot 3 DP 63992, held within Records of Title WN32A/97, WN33A/758 and WN47D/58. The combined area of the site is 2.6197ha. There are a number of easements, including stormwater drainage rights in favour of Council registered on the Records of Title, that will not be impacted by the proposed works.

2.2. The Site

- 11) The site is located at 160 Kāpiti Road, Paraparaumu. It is a regular shaped site fronting Kāpiti Road to the southwest. The site adjoins 11, 15, 21 and 27 Birmingham Street to the north and east, 3, 5 and 7 Arko Place to the south and 180 Kāpiti Road and 17, 19A and 19B Dennis Taylor

Court to the north. The wider area is characterised largely by industrial, heavy commercial, retail and service activities; however, the properties at Dennis Taylor Court are residential properties.

- 12) The old PlaceMakers building will be demolished to make way for the proposed supermarket and onsite car parking area.
- 13) Earthworks will be undertaken to raise the ground level where the building will be located above the 1 in 100-year flood event level. Earthworks undertaken within the proposed car parking area will allow for onsite compensatory storage.
- 14) The surrounding properties have a similar topography to that of the subject site, dropping down from Kāpiti Road before levelling.
- 15) To help provide context to the site in relation to surrounding properties, the aerial photograph below shows the location of neighbouring properties in relation to the site.
- 16) With respect to the receiving environment, attached at **Appendix A** is a list of resource consents received and their status at the time of writing this report, to provide context to those activities in the surrounding environment that are not permitted and may have not been given effect to yet.



Figure 1: Aerial photograph of the entire site. Source KCDC GIS



Figure 2: View from the proposed car parking of the existing building on the site to be removed

2.3. The Proposal

17) Resource consent is sought to:

- Exceed the maximum permitted vehicle movements per day of 200 in the Working Zone. Approximately 400-430 vehicle movements are expected during peak weekday PM and Saturday hours;
- Construct the western access at 14.9m, exceeding the permitted access width of 9m by 5.9m;
- Not provide landscaping between the loading area and adjoining properties;
- Undertake earthworks within the ponding flood hazard area and outside of the ponding flood hazard area, that exceeds the permitted volumes of 20m³ and 50m³ at 2,919m³;
- Construct digital signs of 0.8m², exceeding the permitted maximum size of 0.2m² and that have changeable messaging;
- Construct signs exceeding the permitted 5m² per business/tenancy by 44.2m² for the supermarket and 5.512m² for the two proposed trade retail premises of 400m² located in the northern part of the proposed building. It is anticipated that business operating in these units will be permitted activities;
- Construct a freestanding sign exceeding the permitted height of 6m by 3m and permitted area of 5m² by 24.7m²;
- Construct wall signs exceeding the permitted area of 4m² by 1.1m²;

- Construct parapet signs of 70.2m²;
 - Construct signs that extend beyond the surface area of the fascia;
 - Operate a retail activity that is not ancillary to an industrial activity, in the industrial zone;
 - Construct 200m² of office space which is not ancillary to an industrial activity, in the industrial zone;
 - Operate an off-license premises.
- 18) Hours of operation for business activities that do not adjoin or face the residential zones are not limited by the District Plan. It is anticipated the supermarket will operate 7-days a week between the hours of approximately 6am and 10pm.
- 19) Access to the site will be provided from the existing access via the Kāpiti Road/Friendship Place roundabout, with some widening works proposed.
- 20) The existing second access to the south of the site will be relocated and constructed as a left in, left out access only.
- 21) The applicant has stated that they will accept a condition with respect to compliance with relevant standards for lux levels associated with illumination of the proposed signs noting that no illumination of signs at the rear of the building (facing the residential properties) is proposed.
- 22) A condition is anticipated with respect to the mitigation of the flood hazard and the recommendations of the Civil Engineering Infrastructure Report.
- 23) The applicant has also stated that in addition to sediment and erosion control plans, an appropriately worded condition would be accepted requiring the preparation and provision of a Construction Management Plan and a Construction Traffic Management Plan.
- 24) The applicant proffers a condition with respect to the requirements for a Detailed Site Investigation.

2.4. Application Documentation

- 25) The Application consists of:
- a) Consent Application Forms;
 - b) An Assessment of Environmental Effects dated July 2021 prepared by Forme Planning. This includes 10 Appendices.
 - c) Record of Title (Appendix 1);
 - d) Architectural Drawings (Appendix 2);
 - e) Urban Design Assessment (Appendix 3);
 - f) Landscape Plan (Appendix 4);
 - g) Traffic Assessment (Appendix 5);
 - h) Economic Report (Appendix 6);
 - i) Civil Engineering Infrastructure Report and Drawings (Appendix 7);
 - j) Geotechnical Report (Appendix 8);
 - k) Contamination Report (Appendix 9); and
 - l) Plan Check (Appendix 10).

Further Information

- 26) In accordance with s92(1) of the Resource Management Act 1991 (RMA), the Council requested further information on 27 July 2021, prior to making a notification decision. This request related to:-
- Resolving concerns raised by Ātiawa ki Whakarongotai Charitable Trust with respect to the proposed planting, flood hazard and stormwater;
 - The removal of the Pohutukawa trees located within the berm;
 - The provision of individual services for each tenancy;
 - Traffic modelling.

Peer Review

- 27) A peer review of the Urban Design Assessment was commissioned by Council on 17 August 2021. The peer review was undertaken by Urban Edge Planning Ltd and received by Council on 8 September 2021.

3. KĀPITI COAST DISTRICT PLAN AND NATIONAL ENVIRONMENTAL STANDARDS

National Environmental Standards

- 28) The National Environment Standard for Assessing and Managing Contamination in Soil to Protect Human Health (NESCS) is relevant to this application as the proposal constitutes a change of use. Consent is sought as a **discretionary activity** as a preliminary site investigation (PSI) exists and was submitted with the application. The PSI *confirmed the presence of potential contamination that cannot be determined via on-site testing until the existing building on the site is demolished.*¹
- 29) No other national environment standards are considered to be relevant to this proposal.

Planning Maps

- 30) The site is zoned General Industrial in the Operative District Plan. As mentioned above, part of the site is designated for a plantation reserve, the site is within the Coastal Environment and is subject to the flood hazard ponding over the majority of the area where the new building and car parking area will be constructed.

Relevant Plan Changes

- 31) There are no relevant plan changes applicable to the subject site.

Activity Status

Kāpiti Coast Operative District Plan 2021

Proposed Activity	Rule reference/ description	Activity Status
Vehicle Movements	Rule TR-R10. For a Permitted Activity that does not comply with relevant requirements Rule TR-R2.	Restricted Discretionary Activity
Access and Landscaping	Rule RR-R11. For a Permitted Activity that does not comply with the requirements of Rule TR-R3.	Discretionary Activity
Earthworks in a Ponding Area	Rule NH-FLOOD-R11. For a Permitted Activity that does not comply with the relevant requirements of Rule NH-FLOOD-R4.	Restricted Discretionary Activity

¹ Preliminary Site Investigation Soil and Rock Consultants - page 9

Building Importance Category (BIC) Type 2c Building	Rule NH-EQ-R23. For the construction of a BIC Type 2c building located on sand soils.	Restricted Discretionary Activity
Earthworks outside of the Ponding Area	Rule EW-R5. For a Permitted Activity that does not comply with relevant requirements of Rule EW-R2.	Restricted Discretionary Activity
Signs	Rule SIGN-R11. For Permitted Activities that do not comply with the relevant requirements of Rules SIGN-R1 and SIGN-R8.	Restricted Discretionary Activity
Retail Activities within the General Industrial Zone	Rule GIZ-R22. For a Permitted Activity that does not comply with the relevant requirements of Rule GIZ-R7.	Non-Complying Activity
Offices within the General Industrial Zone	Rule GIZ-R14. For a Permitted Activity that does not comply with the relevant requirements of Rule GIZ-R8.	Non-Complying Activity
Off-license Premises	Rule GIZ-R20	Non-Complying Activity

Overall, resource consent is required from Kāpiti Coast District Council as a **non-complying activity** under Kāpiti Coast District Plan.

4. NOTIFICATION AND SUBMISSIONS

Notification

- 32) Public notification was requested by the applicant, and this occurred on 13 October 2021 in accordance with ss95-95F of the RMA and closed on 10 November 2021.

Submissions:

- 33) A total of three submissions were received by the close of the submission period.
- 34) Submissions were received from the following parties:

Name	Address	Position
1. Modern Merchants Limited	159 Kāpiti Road	Oppose in Part/Neutral in Part
2. Young Supermarkets Limited	159 Kāpiti Road	Oppose in Part/ Neutral in Part
3. Templeton Kāpiti Limited	60 Toru Road	Oppose in Part

- 35) The three submissions received oppose the application, in part, with respect to transportation effects. A full copy of the submissions is provided in **Appendix B** of this report.

5. STATUTORY CRITERIA

- 36) Under s9(3) of the RMA:

No person may use land in a manner that contravenes a district rule unless the use-

- (a) is expressly allowed by a resource consent; or
- (b) is allowed by s10; or

(c) is an activity allowed by s10A.

- 37) The application is for a Non-Complying Activity under the District Plan. The Council may grant or refuse consent under s104B of the Act and, if granted, may impose conditions under s108 of the Act only if it is satisfied that either the adverse effects will be minor, or the activity will not be contrary to the objectives and policies of the District Plan (s104D).
- 38) Section 104(1) of the Act sets out matters a consent authority shall have regard to in considering an application for resource consent and any submissions received.
- 39) Subject to Part 2 of the RMA (Purposes and Principles), the matters relevant to this proposal are:
 - a) *any actual and potential effects on the environment of allowing the activity (Section 104 (1) (a))*
 - b) *any relevant provisions of-*
 - (i) *a national environmental standard:*
 - (ii) *other regulations:*
 - (iii) *a national policy statement:*
 - (iv) *a New Zealand coastal policy statement:*
 - (v) *a regional policy statement or proposed regional policy statement:*
 - (vi) *a plan or proposed plan (Section 104 (1)(b) “*
 - c) *any other matter the consent authority considers relevant and reasonably necessary to determine the application (Section 104 (1)(c))*

5.1. Section 104 Assessment

- 40) The first part of this assessment is to anticipate the effects that the proposal may have on the surrounding environment. This includes positive effects but also particularly the extent or degree to which the proposal would adversely affect the neighbouring properties and wider surrounding environment (Section 6 below).
- 41) The second part of the assessment is to consider whether the proposal is consistent with the outcome sought by any relevant Higher Order Planning documents and is consistent with relevant objectives and policies as set out in the District Plan (Section 7 below).
- 42) The third part of the assessment is to consider whether any other matters are applicable to the proposal (also Section 7 below).

6. EFFECTS (S104(1)(A))

- 43) I have also assessed the types of effects that may arise from the proposed supermarket operations and earthworks. These are:
 - Positive Effects
 - Transport Effects
 - Urban Design Effects
 - Servicing and Infrastructure Effects
 - Natural Hazards Effects
 - Economic Effects
 - Construction Effects
 - Contaminated Soil Effects

- Cultural Effects
- Signage Effects

6.1. Positive Effects

- 44) In terms of the Act the definition of effect under the RMA includes positive effects.
- 45) I turn first to the Applicant's views of the positive effects of the proposal. These are outlined in the AEE² where:-
- *The community benefit resulting from the provision of an additional full-service supermarket within the Paraparaumu community;*
 - *The resultant reduction in travel time for residents to fulfill their grocery needs and a more sustainable travel pattern;*
 - *The activity will be a new employer in the area, both during construction and when operational. Supermarkets of this size typically employ 80-100 staff, being a mix of full-time and part-time employees;*
 - *The proposed supermarket will contribute to the economic development of Paraparaumu as a catalyst for additional investment, as well as the provision of an attractive site development that further interacts and supports the use and attractiveness of the existing services on Kāpiti Road;*
 - *The attractive landscapes street boundary and modern building beyond will contribute positively to the amenity values of the surrounding area, including along the Kāpiti Road frontage; and*
 - *The overall nature of the proposal and its location is convenient and therefore efficient with respect to the market that is proposed to serve.*
- 46) It is also observed that utilising an existing site and removing an existing building that is in a state of disrepair will have advantages for the landowner with an increased return on the leasing of the site.
- 47) It is identified in the Insight Economics assessment³ that there will also be economic benefits including:
- *Customer net benefits - every customer that frequents the new Countdown store must perceive a benefit from doing so, otherwise they would not switch from their existing store.*
 - *Benefits of increased competition - In addition to generating benefits for its own customers, the new store will also benefit the rest of the community by increasing supermarket competition. Given the high share of groceries in the weekly household budget, even a small reduction in average prices will have significant benefits.*
 - *General economic stimulus - construction and operation of the new store will stimulate the local economy. Also, over the longer term, it will provide greater local employment and, in doing so, it may reduce the need for work-related commuting. To that end, it will generate enduring economic and environmental benefits.*
- 48) Overall, there are positive benefits to the Applicant and the community but there are also wider environmental matters that must be considered. These relate to whether the receiving

² AEE Forme Planning - page 34

³ Economics Assessment AEE Appendix 6

environment can accommodate the vehicle movements associated with the proposed supermarket and resulting effects from a potentially out of character retail activity.

6.2. Transport Effects

- 49) The three submissions received (Modern Merchants Limited, Young Supermarkets Limited and Templeton Kāpiti Limited) raised significant concerns with respect to the additional traffic generation from the supermarket impacting the operation of the surrounding road network, including the operation of the Kāpiti Road/Friendship Place roundabout that provides access to the Airport Zone Mixed-Use Precinct and New World Kāpiti.
- 50) The evidence of Mr Trotter, Council's Transport Safety Leader included at **Appendix C** has considered the submissions and the mitigation measures proposed by the Applicant and concludes:

The traffic that will be generated by the proposed supermarket can be accommodated satisfactorily at the modified Friendship Place/Kāpiti Road roundabout and left in/left out Kāpiti Road access without compromising the safe and efficient operation of Kāpiti Road, noting the possible future mitigation of creating a dedicated left turn lane at the western arm of the roundabout should monitoring of the supermarkets traffic effects (as required by consents conditions) determine that these mitigation measures are required. Adequate and appropriate car parking and servicing arrangements can be achieved on the site with minimal impact on surrounding activities. The on-site layout of circulation and parking, including the service area, will comply with the standards in the Kapiti Coast District Plan, and will enable the site to function with minimal impact on the surrounding area.

Upgrading of the crossing points by providing refuge islands at both the roundabout arm into the site and the left in/left out access and provision a pedestrian link across the car park will provide safe access from Kāpiti Road. Alternative travel modes (other than the private car) are also further encouraged by the provision of secure covered cycle parking for customers or staff. The Applicant has indicated that it would be feasible to provide new bus stops on both sides of Kāpiti Road to the west of the left in/left out access. If implemented this would further encourage travel by non-car modes particularly staff employed at the site. The provision of bus stops at this location is supported by Council however, Greater Wellington Regional Council are the approving authority for public transport changes such as new bus stops and the Applicant would need to liaise with them directly to gain their approval, Council can assist with this process.

The Applicant is also proposing a Travel Plan which, if implemented, would assist and support travel to the site by non-car modes. Council is fully supportive of this initiative and a suitably worded consent condition to formalise the travel plan requirement is recommended.

I conclude that the proposed supermarket development is acceptable from an overall transportation point of view, and that the effects will be less than minor.

- 51) I concur with the assessment undertaken by Mr Trotter that with suitable conditions of consent, the transportation effects of the proposal can be mitigated to ensure they are less than minor.

6.3. Urban Design Effects

- 52) The site is highly visible given it adjoins a major community collector (Kāpiti Road), resulting in the potential for adverse streetscape and design effects.
- 53) The application was accompanied by an Urban Design Assessment (UDA) prepared by Richard Knott Limited. The UDA identifies that there are a number of functional and operational requirements to ensure the success of a new supermarket development. Broadly, these relate to the building itself, customer car parking and access and the back of house area for loading and unloading activities.
- 54) In addition to the proposed site layout, the UDA considered four alternative options before concluding that *the proposed development has been designed to appropriately balance the*

opportunities offered by the site and the requirements for supermarket design, whilst also taking account of the expectations of the Council's Streetscape Strategy and Guidelines, the Crime Prevention Through Environmental Design Guidelines and Policy 6.16 – Land Use and Built Form in the General Industrial Zone. It represents an appropriate urban design and landscape response to the context and which will ensure that the development will be integrated into its surroundings.⁴

55) In this case, the opportunities offered by the site are identified as:

- *Places the supermarket, and trade retail, buildings closer to the back of the site in order to accommodate easy to view and easy to use car parking between the proposed building and the Kāpiti Road site frontage.*
- *Brings forward well designed modern building facades towards Kāpiti Road, which are of a scale which will provide a presence to the street and presents an overall appearance which is of significantly greater interest and of much improved design relative to the building existing on the site, to the benefit of the streetscape in general.*
- *Provides large areas of glazing facing the car park, in both the supermarket, online area and trade retail buildings, ensuring good passive surveillance of the car park and towards Kāpiti Road.*
- *Includes an additional 200m² of floor area dedicated to fulfilling online orders. This space has a frontage to the car park with immediate access to dedicated pick-up car spaces provided under a canopy. The provision of this space will bring greater activation to the front of the supermarket building.*
- *The provision of car parking between the supermarket and trade retail frontages and the street reflects the current pattern of development seen in the area and will ensure that there is good quality access from the car park to the store frontages.*
- *Service yards for the supermarket and trade retail space are provided away from public view and away from sensitive used (such as residential which are far removed to the north-east), at the rear of the trade retail buildings and to the side of the supermarket. These spaces can be made secure and have been designed to allow for the safe manoeuvring of delivery vehicles. Separating these areas entirely from publicly accessible areas will assist the operators to meet their health and safety obligations.*
- *Provides high quality landscaping along the site frontage and boundaries, as well as new street trees (supplementing those already within Kāpiti Road), which together break/layer views of the buildings from Kāpiti Road, to be benefit of the street scene in general.⁵*

56) A peer review of the UDA was commissioned pursuant to section 92(2) of the RMA. This was undertaken by Jaime Devereux of Urban Edge Planning Ltd. The review found that the UDA could largely be agreed with:

With regard to the proposal being functional and the associated constraints associated with maximising the site for supermarket use. There has been a typical approach taken to the supermarket design (including building and siting) which prioritises the motor vehicle consumer. I do want to emphasise that the proposal does not represent good urban design and that the perception of supermarkets and how they are used will not change without innovative precedence. The proposal is entirely vehicle dominated and presents a large expanse of asphalt fronting the street. I know that creative design solutions can allow for successful supermarkets

⁴ Urban Design Assessment Richard Knott Limited - page 21

⁵ Urban Design Assessment Richard Knott Limited - pages 9 and 10

that integrate better, with an attractive and inviting frontage with a pedestrian focus; especially given this site is located within walking distance from residential areas.

In saying that, I can also acknowledge that the site is zoned for industrial use, where this sort of building mass and car parking arrangement is widely accepted, both within the District Plan and from the public. It will not be out of character with the existing environment in that regard.

It is good to see that the proposal has incorporated space for electric cars and a covered pedestrian walkway from the public footpath. There has been a comprehensive assessment provided on how the proposal has achieved CPTED principals. The proposed landscaping is minimal however, and nothing has been done to break up the car parking area with surface variation as discussed at the pre-application meeting. To be able to provide support for the proposal I would like to see more done with the car parking area to soften and break up the large area of asphalt.

I would like it noted that there has been a missed opportunity with this proposal and it is only the fact that it is located within an industrial zone where development of this form and building siting is generally more accepted that it is possible to provide urban design support.⁶

- 57) In line with the above review, if consent is granted, I recommend conditions with respect to the preparation of a landscape plan utilising species native to the Foxton Ecological District within which the Kāpiti Coast is located and the breaking up of the car park asphaltic expanse by using changing materials/surface treatments.

6.4. Servicing and Infrastructure Effects

- 58) Council's reticulated water and wastewater services are available to the site from within Kāpiti Road and Birmingham Street. The Infrastructure Report submitted with the application concluded that there was sufficient capacity within these systems to provide for the new supermarket and trade retail premises.
- 59) The onsite wastewater system will be upgraded by the provision of two new private pump stations and replacement of parts of the current system will occur as required.
- 60) Council's Development Engineer requested as part of the s92(1) that individual service connections were provided for each of the three proposed tenancies as shown on the plans. The response from the Applicant received 9 August 2021 provided updated plans showing the individual connections.
- 61) If consent is to be granted, I recommend conditions with respect to the construction of the two new pump stations, the requirement for individual service connections for each tenancy and the requirement for CCTV footage of the existing 80mm AC Rising Main to determine the material and condition of the pipe and require replacement with a new PE pipe if footage shows the existing pipe is not suitable.

6.5. Natural Hazards Effects

- 62) As shown in Figure 3 below, the site is subject to the flood hazard ponding.

⁶ Urban Design Assessment Review urban Edge Planning Ltd - page 1

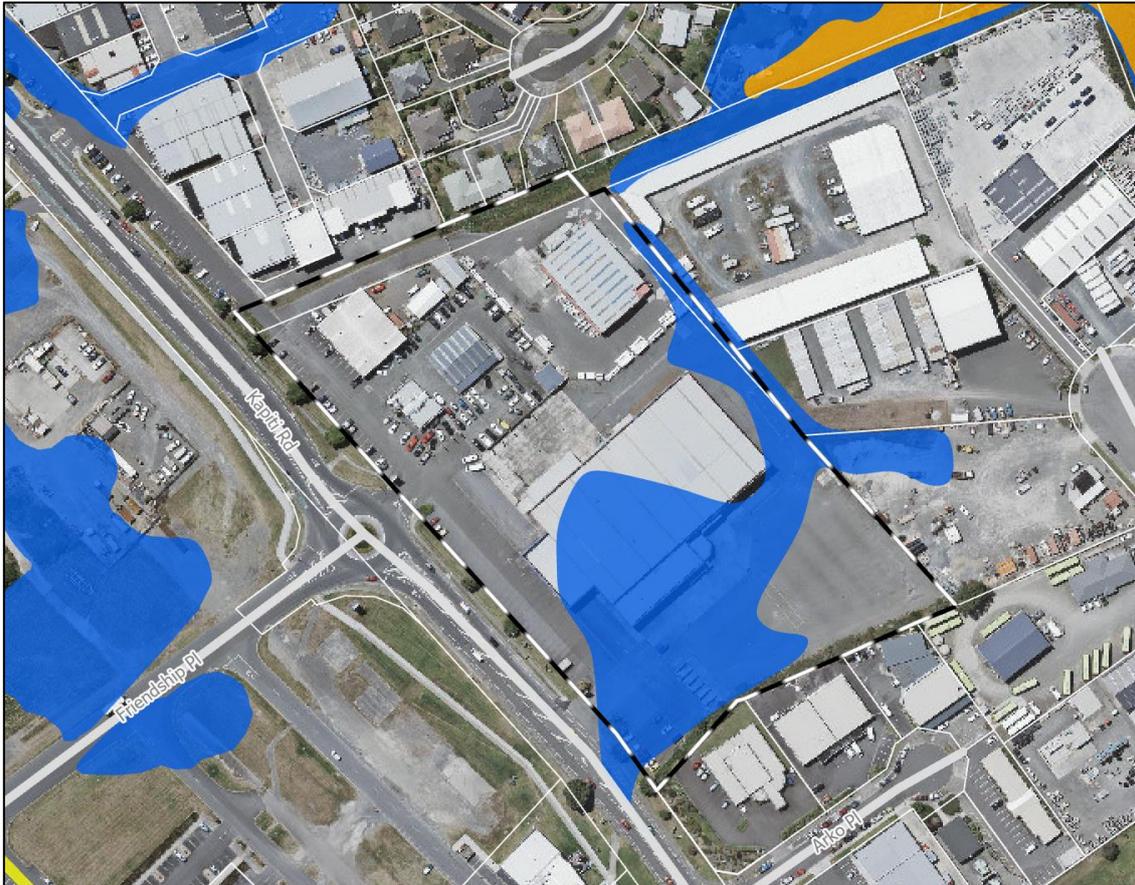


Figure 3: Area of site subject to the flood hazard ponding shown in blue. Source KCDC ePlan

- 63) As detailed in the Infrastructure Report prepared by Maven Associated, earthworks are proposed to mitigate this flood hazard to provide a flood free building area.
- 64) Earthworks within flood hazard areas have the potential to cause displacement effects and result in increased flooding on other properties within the flood catchment. The catchment area for the ponding flood hazard is largely within the subject site and to avoid increasing flooding on adjoining sites, it is proposed to maintain flood storage capacity within the site, by providing compensatory storage on the site.
- 65) The Infrastructure Report prepared by Maven Associates states that modelling shows a volume of 1,140m³ of ponding on the site in a 1 in 100-year flood event that will be distributed between three storage areas. Onsite storage will be provided in the sealed car park area and a non-public area at the rear of the supermarket building ranging in depths of 150mm (in the car park) to 230mm (non-public areas) and able to contain an estimated 340m³ of water. The additional 900m³ of displaced flood water will be contained within underground flood storage tanks located beneath the car park area.
- 66) The District Plan requires resource consent for the construction of a BIC Type 2c building located on sand soils.
- 67) A geotechnical report was submitted with the application which determined, based on existing geotechnical data that the inferred soil conditions are likely to be interbedded medium dense to dense sands and silty sands to greater than 7m below ground level.
- 68) Onsite testing opportunities are limited given the site requires consent under the NESCS to disturb the soil; however, the report identifies that the site is likely to be prone to earthquake induced liquefaction and therefore foundation design details to mitigate this will require site specific geotechnical investigations.

- 69) If consent is to be granted, I recommend conditions to ensure the construction of onsite flood storage is undertaken in accordance with the details provided in the Infrastructure Report and the provision of and compliance with a site-specific geotechnical investigation that includes foundation design measures to mitigate liquefaction risk to ensure less than minor natural hazard effects.

6.6. Economic Effects

- 70) Appendix 6 of the application is an Economic Assessment of the proposal undertaken by Insight Economics Limited. Prior to lodgement, as agreed by Council and the Applicant, a review of this assessment was undertaken by Property Economics Limited. The conclusions of the report were supported by Property Economics Limited in that:

The results confirm that the proposed new Countdown store will affect the turnover of food retailers across the district to varying degrees (in direct proportion to their proximity). For example, the greatest trade impacts (20%) will be felt by the New World store at Kāpiti Landing, and the nearby Four Square store on Kāpiti Road, whose trade catchments have the greatest overlap with the proposed new Countdown store. As a result, a significant share of the new store's trade impacts will be absorbed by businesses located outside the district's centre hierarchy. This, in turn, will help to avoid any adverse economic effects arising from the proposal.

The next greatest effects (16%) will be felt by the handful of small food retailers at the Paraparaumu Beach town centre, as it is only 1.6 kilometres from the site. Finally, the analysis estimated that PPTC supermarkets would experience trade impacts of 14%, which equates to approximately 6% of total centre sales.

Having considered the likely impacts of the proposal in detail, we do not consider it to pose a risk of significant adverse retail distribution effects to the PPTC for several reasons, namely that:

- Trade impacts will be spread across a strong and diverse network of existing food retailers, not just shouldered by the two supermarkets at the PPTC. In fact, the worst impacts will be felt by two out-of-centre stores, which helps insulate other supermarkets (and their centres) from adverse effects.*
- In addition, the proposal's highly-visible and easily-accessible location will draw customers from a wide geographic catchment, which further helps to diffuse trade impacts. As a result, no stores will close because of the proposal, which dramatically reduces the scope for any adverse retail distribution effects to occur.*
- This conclusion is further reinforced by projected growth in district retail demand, which will ensure that trade impacts are not only relatively minor, but also short-lived.*
- It is also strengthened by the fact that Woolworths NZ is committed to maintaining and improving its presence in the PPTC via its existing Countdown store there. This would not be the case if they foresaw significant detriment from the proposed new Countdown.*
- Trade impacts are limited only to one PPTC store type (supermarkets), with no impacts occurring on its other retail store types, nor its various other non-retail tenants.*
- The draft master plan for the PPTC includes several initiatives that will improve its attractiveness to a wide range of customers, and hence ensure its future health and vitality.*
- People who previously shopped at specialty stores in Coastlands before or after a supermarket visit will still return to those stores even if they no longer frequent its supermarkets, because they remain the best way to meet those specialty retail needs.*

Accordingly, we do not believe that the proposal will have any significant adverse effects on the PPTC, and instead will help make the district's retail network stronger and more diverse.

In addition to retail distribution effects, we also considered possible adverse effects of the proposal on the district's supply of industrial land (because the land is zoned General Industrial under Operative District Plan 2021 (ODP 2021)). However, our analysis showed that vacant industrial land can provide additional floorspace that is eight times greater than our estimates of additional demand to 2043, while the district's own forecast is for industrial floorspace demand to decline, not grow, over time. Accordingly, the proposal will not have any adverse effects on the efficient functioning of the district's industrial land market either. Moreover, the proposal is consistent with relevant ODP 2021 objectives and policies related to non-industrial uses of General Industrial zoned land.

- 71) It is my view that any economic effects of the proposal are less than minor.

6.7. Construction Effects

- 72) As outlined in the AEE, site works, and construction of the supermarket building could take up to eight months⁷.
- 73) Construction has the potential to result in adverse effects with respect to noise, dust and construction vehicle movements. The application states that due to the nature of the site, construction methodologies such as rock blasting or drilling will not be necessary and the permitted activity standards in the ODP relating to noise will be met.
- 74) The Applicant has volunteered a condition with respect to the provision of Construction Management and Construction Traffic Management Plans to mitigate the construction effects of the proposed works to ensure they are less than minor.
- 75) I concur that with the provision of a Construction Management Plan, that includes measures relating to construction traffic, submitted for approval by Council's Development Engineer prior to the commencement of works, that the adverse construction effects will be less than minor.

6.8. Contaminated Soil Effects

- 76) The site is not identified on the Greater Wellington Regional Council's Selected Land Use Register; however, activities being undertaken on the site are identified on the Ministry for the Environment's Hazardous Industries and Activities List. To this end, a Preliminary Site Investigation (PSI) was submitted with the application.
- 77) The PSI includes a detailed summary of previous land uses:

Based on the information obtained and reviewed as part of this investigation, we understand that the site has historically been used for pastoral operations prior to the late 1980s. After which time the site was developed for commercial purposes and several new structures constructed. The structures included a moderately sized warehouse structure occupied by Placemakers and used for the sales and storage of treated timber as well as two other structures used by Capital City Ford as car dealership/service centre and another structure used by Carpet Court (formerly Repco) for the sale of residential and commercial carpets sales.

Due to the nature of the historical and current operations, Heavy Metals (HM), Polycyclic Aromatic Hydrocarbons (PAH), Total Petroleum Hydrocarbons (TPH) and/or Pentachlorophenol (PCP) associated with the storage of fuel/oils as well as treated timber may be present in soils in particular within the vicinity of the former timber yard and automotive service centre.⁸

⁷ AEE Forme Planning - page 41

⁸ Preliminary Site Investigation Soil & Rock Consultants – page 8

- 78) The PSI concludes:

Contamination of site soils in regard to risk to human health is possible based on historical land-use and activities of which the concentration and extent could not be ascertained without further investigation and analysis of site soils.

A DSI to ascertain the potential and/or extent of contamination present in the soils is been recommended. This recommendation is based on the findings of this report and available data, which has identified activities or land-use which could lead to the possibility of subsoil contamination.⁹

- 79) In line with the conclusions and recommendations of the PSI, if consent is granted, it is recommended conditions are included with respect to the preparation of the DSI following the removal of the existing building and provision of remediation plans being submitted to Council for approval. The inclusion of these conditions will ensure the effects are less than minor.

6.9. Cultural Effects

- 80) As the application involves earthworks, as part of Council's Memorandum of Understanding, it was provided to local iwi Ātiawa ki Whakarongotai for review.
- 81) The report provided by the Ātiawa ki Whakarongotai Charitable Trust (the Trust) raised concerns with respect to the underground flood storage tanks and potential impacts on groundwater and the nearby Wharemaukū Stream, confirmation on the species of trees to be planted on the berm with the request they are native species and the potential for sediment and untreated stormwater from the proposed earthworks to enter the Wharemaukū Stream.
- 82) The s92(1) request included a requirement for the Applicant to address the concerns raised by the Ātiawa ki Whakarongotai Charitable Trust.
- 83) The Trust was satisfied with the response by the Applicant, submitted to Council on 9 August 2021, provided the Erosion and Sediment Control Plan prepared by Maven Associates and requirement for the berm trees to be native species formed conditions of consent if consent was granted.
- 84) If consent is to be granted, I recommend conditions are included requiring compliance with the Erosion and Sediment Control Plan and planting of native species for any landscaping associated with the works, including any berm planting.

6.10. Signage Effects

- 85) The proposal includes a number of non-compliances with respect to the proposed signage. Some of these, such as the signs relating to grocery collection where collection has been undertaken by supermarket staff, are internal to the site and will not be visible from any adjoining site or public place. Signs such as these are considered secondary signs and will have less than minor effects with respect to not complying with the permitted activity standards.
- 86) Primary signage for the proposal includes directional signs located at the entrances to the site necessary to ensure vehicles follow the access requirements, particularly with respect to left in/left out access. These directional signs will have a less than minor effect and are necessary for safety reasons.
- 87) The façade of each premises will have signage containing the name of the business. As the business that may occupy the proposed trade premises are not known, there is no further detail available on this. Façade signage for the supermarket will contain the name Countdown and be in the corporate green, white and grey colouring. Given the setback of the buildings from Kapiti

⁹ Preliminary Site Investigation Soil & Rock Consultants – page 9

Road, it is not considered that this signage will dominate the streetscape and therefore the effects are considered to be less than minor.

- 88) The freestanding sign located at the Kapiti Road/Friendship Place entrance is 3m higher and 24.7m² larger than the freestanding signs permitted under the District Plan in this location. The sign proposed will allow motorists to have ample time to make manoeuvring decisions, reducing the potential for traffic incidents to occur within the road carriageway. The freestanding sign will be consistent with other freestanding signs located within the surrounding environment. Wording and images on the freestanding sign are limited to the business name, logo, opening hours and services that will be located within the supermarket at a scale that does not cluster the sign or have the potential to cause a distraction to drivers.
- 89) Signage is considered to form part of the character associated within the Working Zones, although there are a number of non-compliances with respect to the signage proposed, the balance of this between internal signage not visible until you are well within the site, the signage necessary for transport safety and signage that is typical for supermarkets is considered to have a less than minor effect. There are not considered to be any cumulative effects associated with the proposed signage in this location.
- 90) Illumination of the signage will comply with the requirements of the District Plan with respect to the requirements of the General Industrial Zone.

6.11. Conclusions as to Effects

- 91) There are clear positive effects in that the proposal provides for the wider wellbeing of the community as it provides services and amenities, in close proximity to residential areas and it will revitalise a currently underutilised site.
- 92) However, there are adverse effects, specifically those relating to transportation that have been well outlined in the three submissions received. The adverse transportation effects can be mitigated by the imposition of conditions recommended by Mr Trotter.
- 93) The adverse effects relating to earthworks, urban design, construction and contaminated soils can also be mitigated by the imposition of the volunteered conditions or those included in **Appendix D**.

7. STATUTORY ASSESSMENT S104(1)(B)

- 94) The application correctly identifies the majority of relevant Objectives and Policies within that are applicable.

7.1. Higher Order Documents

- 95) s104(1)(b) of the Act requires that a decision maker must have regard to any relevant provisions of:
 - (i) **a national environmental standard:**
 - 96) **The National Environment Standard for Assessing and Managing Contamination in Soil to Protect Human Health** is considered to be relevant to this application. No other National Environmental Standards are applicable.
 - (ii) **other regulations:**
 - 97) None are applicable.
 - (iii) **a national policy statement:**
 - 98) **The National Policy Statement on Urban Development 2020** (NPS-UD) requires local authorities to provide sufficient development capacity in their resource management plans, supported by infrastructure, to meet demand for housing and business space. In order to assess

forecasted demand and associated capacity, Wellington City Council, Hutt City Council, Upper Hutt City Council, Porirua City Council and Kāpiti Coast District Council cooperatively prepared a report titled 'Housing and Business Development Capacity Assessment' (HBA), which was released on 8 November 2019 (under the previous iteration of the NPS-UD, being the National Policy Statement on Urban Development Capacity 2016).

- 99) The NPS-UD identifies the Kapiti Coast District Council as a Tier 1 local authority (high growth) with the Housing and Business Development Capacity Assessment (HBA) identifies that based on 2017 population forecast projections, the Kāpiti Coast District would see a 26% increase in population over the 30-year period covered by the HBA (2017-2047).
- 100) The key findings relating to future business demand relevant to the proposed supermarket are:
- *Kāpiti will require an additional 61,585m² of floorspace to accommodate future demand for business activities.*
 - *The largest demand for floorspace relative to an increasing need for healthcare services; with modest increases to retail floorspace, and minor increases in floorspace for commercial and government activities.*
 - *Future demand for industrial land is expected to decline as a reflection of the shift from heavier industries to more service-based activities, smaller scale manufacturing and less land-intensive activities.*
 - *Kāpiti District's demand projections are sensitive to population growth. Higher population growth will increase future business demand across all sectors.¹⁰*
- 101) Given population growth with result in an increase in demand for services, the proposed supermarket and trade retail premises are considered to aid in Council meeting the requirements of the NPS-UD.

(iv) a New Zealand coastal policy statement:

- 102) This is not directly applicable as the site is not considered to contain any Coastal Environment features, but it is acknowledged that the District Plan maps identify the site as being within the Coastal Environment.

Regional Policy Statement for the Wellington Region 2013 (RPS)

- 103) The RPS outlines the resource management issues of significance to the region and provides a framework for managing the natural and physical resources of the region in a sustainable manner. Further to this, the RPS identifies objectives, policies and methods which are designed to achieve integrated management of the natural and physical resources of the whole region.
- 104) Chapter 3 of the RPS outlines the regionally significant resource management issues and the objectives of the RPS and Chapter 4 outlines the policies and methods to achieve these. Section 4-2 outlines the regulatory policies to be considered when assessing and deciding on resource consent applications.
- 105) Objectives and Policies relevant to this application are discussed under the following headings:
- Natural hazards
 - Regional form, design, and function
 - Resource management with Tāngata Whenua

Natural hazards

Policy 51 seeks to minimise the risks and consequences of natural hazards.

¹⁰ Housing and Business Land Capacity Assessment Kāpiti Coast District Council - page 260

- 106) The proposed earthworks will minimise the risk from flooding while not increasing flooding on adjoining properties by compensatory storage capacity being provided for onsite.
- 107) The risk from earthquake induced liquefaction will be mitigated through foundation design for the proposed supermarket building.

Regional form, design, and function

Objective 22 relates to the provision of compact, well designed, and sustainable regional form that has an integrated, safe and responsive transport network.

Policy 54 seeks the achievement of the region's urban design principles.

Policy 57 seeks for the integration of land use and transportation.

Policy 58 seeks for the co-ordination of land use with development and the operation of infrastructure.

- 108) The supermarket is located on an existing, underutilised site that is well serviced by public transport and the existing road network which will undergo some minor upgrades to facilitate the increased use of the site.

Resource management with Tāngata Whenua

Policy 48 provides for the principles of the Treaty of Waitangi

Policy 49 seeks to recognise and provide for matters of significance to tāngata whenua

- 109) Ātiawa ki Whakarongotai Charitable Trust were provided with a copy of the application by KCDC, upon lodgement. As part of the response to the s92(1) request, the concerns raised were resolved.

Kāpiti Coast District Plan

- 110) The Kāpiti Coast District Plan was made operative on 30 June 2021. There are applicable relevant objectives and policies relating to:

- District-Wide Matters relating to Infrastructure
- District-Wide Matters relating to Transport
- District-Wide Matters relating to Contaminated Land
- District-Wide Matters relating to Natural Hazards
- General District-Wide Matters relating to Earthworks
- General District-Wide Matters relating to Signs
- Area Specific Matters relating to the General Industrial Zone
- District-Wide Matters relating to Strategic Direction

Infrastructure

Objective DO-O3 – Development Management To maintain a consolidated urban form within existing urban areas and a limited number of identified growth areas which can be efficiently serviced and integrated with existing townships and infrastructure.

Objective DO-O13 – Infrastructure To recognise the importance and national, regional and local benefits of infrastructure and ensure the efficient development, maintenance and operation of an adequate level of social and physical infrastructure and services throughout the District.

Policy INF-GEN-P1 – Recognition To recognise the national, regional or local importance and benefits of sustainable, secure and efficient provision of infrastructure.

Policy INF-GEN-P4 – Managing Adverse Effects To avoid, remedy or mitigate adverse environmental effects arising from the establishment, operation, maintenance and upgrading of infrastructure.

Policy INF-GEN-P7 – Infrastructure and Growth Management To focus subdivision, use and development of land for urban growth and intensification to certain areas (i.e., existing urban areas).

Policy INF-GEN-P11 – Quality of Infrastructure Design and Services To provide infrastructure for subdivision and development in accordance with the Council’s Subdivision and Development Principles and Requirements (SDPR) 2012.

Policy INF-MENU-P17 – Hydraulic Neutrality – Stormwater To ensure stormwater from new impermeable surfaces from subdivision and development will be disposed of or stored onsite and released at a rate that does not exceed the peak stormwater runoff when compared to the pre-development situation.

INF-MENU-P18 – Stormwater Quantity and Quality To minimise in particular cumulative effects of adverse effects of stormwater runoff from subdivision and development.

INF-MENU-P20 – Water Supply To ensure all new subdivision, land use and development has an adequate supply of water, including firefighting supply.

INF-MENU-P21 – Wastewater To ensure adequate treatment and disposal of wastewater from subdivision, land use and development that avoids, remedies or mitigates adverse effects on the environment and maintains public health and safety.

- 111) The proposal is consistent with the intent of the objectives and policies identified above. Each premises will have individual water and wastewater connections as required by Council’s SDPR.
- 112) Onsite stormwater disposal will be provided and ensure that the site is hydraulically neutral noting that the area of the site where works are proposed is currently largely impermeable and will remain impermeable, as shown on the plans submitted with the application.

Transport

Objective DO-O3 – Development Management To maintain a consolidated urban form within existing urban areas and a limited number of identified growth areas which can be efficiently serviced and integrated with existing townships and infrastructure.

Objective DO-O8 – Strong Communities To support a cohesive and inclusive community.

Objective DO-O14 – Access and Transport To ensure the District has a transport system the provides for the integrated movement of people, goods and services.

Objective DO-O15 – Economic Vitality To promote sustainable and on-going economic development of the local economy.

Policy TR-P1 – Integrated Transport and Urban Form Ensure development and subdivision will be integrated with and consistent with the transport network hierarchy.

Policy TR-P2 – Sustainable Transport and Maximising Mode Choice Ensure development and subdivision will be integrated with a transport system that offers a wide range of travel mode choices, connecting residents to essential community services, centres, and social infrastructure.

Policy TR-P3 – An Efficient and Economic Transport Network Increase the economic vitality of the District by the development, operation, maintenance and upgrading of the transport network.

Policy TR-P4 – Effects of Transport on Land Use/Development Avoid, remedy, or mitigate the potential adverse effects of development, operation, maintenance and upgrading of the transport network on land use and development.

Policy TR-P5 – Effects of Land Use on Transport Avoid, remedy, or mitigate the potential adverse effects on the transport network from development and subdivision by identifying key existing transport routes.

Policy TR-P6 – Safety Ensure the safety of all transport users will be enhanced during the development, operation and upgrading of the transport network.

Policy TR-P7 – Cycling, Walking and Bridleway Links and Safety Ensure subdivision, use and development are located and designed to make walking, cycling and the use of bridleways safer, more enjoyable and convenient.

- 113) Upgrades to the Friendship Place/Kapiti Road roundabout are proposed to ensure that the intersection and access to Kapiti Landing across Kapiti Road retain efficiency. These upgrades will also mitigate the effects of the proposed development on transport and support the safety of the road network.
- 114) A shared pathway is provided along the western side of Kapiti Road while the eastern side where the site is located has a footpath and wide berm. These will provide adequate access to the site for pedestrians and cyclists.
- 115) A walkway is provided from the Kapiti Road footpath to the front entrance of the supermarket.
- 116) The 260 Bus Route is available within Kapiti Road with the nearest bus stops located on either side of Kapiti Road approximately 285m to the northwest of the site at Lodestar Place. Busses are available at these stops every 20-60 minutes all day.
- 117) Covered parking for bicycles will be provided at the front of the store. Mobility, electric vehicle, parent, and motorbike parking will also be provided.

Contaminated Land

Objective DO-O10 – Contaminated Land To prevent or mitigate any adverse environmental effects, including risks to human health and the environment, arising from past, present or future activities involving contaminated land.

Policy CL-P1 – Identify Contaminated and Potentially Contaminated Land Identify contaminated and potentially contaminated land through the resource consent or plan change processes and enable the land to be managed or remediated to eliminate any unacceptable risk to the environment.

- 118) A PSI has identified potential contamination of the site with a DSI proposed following removal of the existing building. Conditions of consent will ensure remediation of the site in a way that does not increase the risk to people, or surrounding properties.

Natural Hazards

Objective DO-O3 – Development Management To maintain a consolidated urban form within existing urban areas and a limited number of identified growth areas which can be efficiently serviced and integrated with existing townships that does not result in the increase in risk to life or severity of damage to property from natural hazard events.

Objective DO-05 – Natural Hazards To ensure the safety and resilience of people and communities by avoiding exposure to increased levels of risk from natural hazards, while recognising the importance of natural processes and systems.

Policy NH-P1 – Identify Hazards The extent of flooding and seismic hazards will be identified on the District Plan Maps.

Policy NH-P2 – Risk Based Approach A risk-based approach will be taken to subdivision, land use and development within areas subject to flood, fire and earthquake hazards.

Policy NH-P3 – Managing Activities in Natural Hazard Prone Areas In areas identified on the District Plan Maps, new subdivision, use and development will be managed in a way that

avoids increasing risks from natural hazards. Subdivision, use and development will be allowed only where it can be shown that any potential increase in risk exposure on or beyond the land itself has been avoided, remedied, or mitigated.

Policy NH-FLOOD-P8 – Flood Mapping Map flood hazard categories using the 1% AEP flood modelling scenario. The extents and categories consider projected climate change and precautionary freeboard to minimise risks.

Policy NH-FLOOD-P11 – Flood Risk Levels To place a higher level of control on subdivision, use and development within river corridors, stream corridors, overflow paths and residual overflow paths areas. Apply a lesser level of restriction on subdivision, use and development within ponding, residual ponding, shallow surface flow, flood storage and fill control areas.

Policy NH-FLOOD-P13 – Ponding, Residual Ponding, Shallow Surface Flow, Flood Storage and Fill Control Areas To consider effects, including displacement effects of subdivision, use and development within a ponding, residual ponding, shallow surface flow, flood storage or fill control area.

- 119) The proposed earthworks will mitigate the ponding flood hazard with respect to the location of the proposed supermarket. The flood waters in a 1% AEP event will be diverted to storage areas within the carpark and at the rear of the proposed building. Onsite storage will mean there is no increase in risk exposure beyond the site itself as no displacement of flood waters is occurring beyond the site.
- 120) The risk from earthquake induced liquefaction will be mitigated by the building having specifically designed foundations.
- 121) The proposal is consistent with the objectives and policies relating to natural hazards.

Earthworks

Objective DO-O1 – Tāngata Whenua To work in partnership with the Tāngata Whenua of the District in order to maintain kaitiakitanga of the District's resources and ensure that decisions affecting the natural environment in the District are made in accordance with the principles of Te Tiriti o Waitangi (Treaty of Waitangi).

Objective DO-O11 – Character and Amenity Values To maintain and enhance the unique character and amenity values of the District's distinct communities.

Policy EW-P1 – Earthworks To ensure that earthworks are designed to be sympathetic to the natural topography.

- 122) Much of the subject site is currently impermeable with limited areas of vegetation and grass. The vegetation within the site is not maintained. It is considered that there are limited amenity values to the site as it is currently, given the state of disrepair the existing building is in. The proposal will enhance the character and amenity of the site by way of a modern designed building.
- 123) Tāngata Whenua have reviewed the application to ensure works are in accordance with the principles of Te Tiriti o Waitangi. The concerns raised were resolved as part of the section 92(1) further information request.
- 124) While earthworks will be undertaken over a large area of the site, they have been designed to retain the slope into the site from Kapiti Road, maintaining as much as possible the current site characteristics.

Signs

Objective DO-O3 – Development Management To maintain a consolidated urban form within existing urban areas and a limited number of identified growth areas which can be efficiently serviced and integrated with existing townships that provides a variety of areas for business/employment.

Objective DO-08 – Strong Communities To support a cohesive and inclusive community.

Objective DO-011 – Character and Amenity Values To maintain and enhance the unique character and amenity values of the District's distinct communities.

Objective DO-015 Economic Vitality To promote sustainable and on-going economic development of the local economy, including the rural sector, with improved number and quality of jobs and investment.

Policy SIGN-P1 Public Benefit to Recognise the public benefit of appropriately designed and located signs.

Policy SIGN-P2 Character and Amenity of the Residential and Rural Zones Manage the type, size, location, and design of signs to protect the landscape, character and amenity values of the Residential and Rural Zones.

Policy SIGN-P3 Character and Amenity of the Working Zones Manage the type, size, location, and design of signs to enable business to identify and advertise their business premises while minimising any adverse effects of signage on the landscape, character, and amenity values of the Working Zones.

Policy SIGN-P4 – Safety to ensure signs are designed and located to not interfere with the safe and efficient use of road and pedestrian/cycle ways.

Policy SIGN-P5 – Sign Assessment Criteria In assessing applications for signs not meeting the permitted activity standards, have regard to the purpose, location, character and amenity, type, safety and cumulative effects to determine the appropriateness or otherwise of the proposed sign.

- 125) The proposed signs required for the supermarket and trade premises have public benefit to ensure customers can find the services they are looking for.
- 126) Signs within Working Zones, and on Kapiti Road in particular, are not considered to be out of character. A range of services are provided within the site and to ensure safety for surrounding pedestrians and drivers, the signs need to be of a size where messages are clearly visible.
- 127) The signs will not be readily visible from any surrounding residentially zoned properties.
- 128) A number of the proposed signs are located within the site itself and given the nature of the surrounding environment will not be readily visible from the road carriageway and therefore will not create a safety risk.

General Industrial Zone

Objective DO-03 – Development Management To maintain a consolidated urban form within existing urban areas and a limited number of identified growth areas which can be efficiently serviced and integrated with existing townships that provides a variety of areas for business/employment.

Objective DO-08 – Strong Communities To support a cohesive and inclusive community.

Objective DO-011 – Character and Amenity Values To maintain and enhance the unique character and amenity values of the District's distinct communities.

Objective DO-015 Economic Vitality To promote sustainable and on-going economic development of the local economy, including the rural sector, with improved number and quality of jobs and investment.

Policy GIZ-P1 Non-Industrial Activities in the General Industrial Zone Prevent the uptake of industrial land by managing residential activities, retail activities (except trade supply retail and yard-based retail) and commercial activities which are not ancillary activities to a primary industrial activity.

Policy GIZ-P3 Industrial Fringe At the interface between the General Industrial Zone and neighbouring zones manage building bulk, outdoor storage, signage, noise and vibration to minimise adverse effects.

GIZ-P4 Activities in the Working Zones Provide for business activities and the primary land use and function of the Working Zones.

- 129) The proposal is to undertake business activities within a Working Zone which is in accordance with Policy GIZ-P4 and the site is located within an existing urban area.
- 130) Although the proposal includes retail activities that are not ancillary to an industrial activity, the site is considered appropriate for the proposed use, given the proximity to residentially zoned areas resulting in less time spent travelling for vital amenities.
- 131) The site is considered to be located within the Industrial Fringe and the operation of a retail activity on the site is considered more appropriate in close proximity to residentially zoned areas than heavy industrial activities which involve the processing of raw materials, commonly with significant machinery that creates adverse noise effects.
- 132) The proposal to redevelop a currently underutilised site will provide economic vitality for the District.

Strategic Direction

Objective DO-O3 – Development Management To maintain a consolidated urban form within existing urban areas and a limited number of identified growth areas which can be efficiently serviced and integrated with existing townships that provides a variety of areas for business/employment.

Objective DO-O8 – Strong Communities To support a cohesive and inclusive community.

Objective DO-O11 – Character and Amenity Values To maintain and enhance the unique character and amenity values of the District's distinct communities.

Objective DO-O15 Economic Vitality To promote sustainable and on-going economic development of the local economy, including the rural sector, with improved number and quality of jobs and investment.

Policy BA-P1 – Management of Business Activities Manage business activities within the District to enable consolidation, efficiency and integrated areas within identified zones and precincts.

Policy BA-P3 – Consolidation of Business Activities Consolidate business activities in centres and other Working Zones.

Policy UEDI-P1 – Urban Design Promote quality urban design outcomes.

Policy UEDI-P2 – Safety and Crime Prevention Through Environmental Design To enhance the safety and security of residents and visitors by ensuring development, use and subdivision is consistent with the Subdivision and Development Principles and Requirements 2012 and Crime Prevention Through Environmental Design (CPTED) Guidelines.

- 133) Sites of a size suitable for the construction of new supermarkets are typically limited within existing urban areas and construction outside of these areas has the potential to create urban sprawl. The development will be constructed on an existing site,, that it not currently being used to its full potential, in an area where sites are predominantly used for compatible business activities.
- 134) As detailed in the Economics Assessment provided with the application, the proposal will stimulate the local economy and provide new employment opportunities.

- 135) The site design considered CPTED and where additional urban design enhancements can be made, these are recommended as conditions of consent to ensure quality urban design outcomes.

Conclusions as to Statutory Framework s104(1)(b)

- 136) I have considered the specific provisions of the relevant policy statements and plans. While there is some policy support for the proposal there is no one defining matter. A number of relevant matters in the objectives and policies concern the management of adverse effects.
- 137) It is considered that the application is more about avoiding and mitigating adverse effects rather than being specifically contrary to the range of objectives and policies that are applicable.

Any Other Matters (s104(1)(c))

- 138) There are no other matters considered relevant to assist in determining this application.

8. ASSESSMENT UNDER PART 2 OF THE RMA 1991

- 139) Part 2 of the Act sets out the purpose and principles of the legislation, which as stated in s5, is “to promote the sustainable management of natural and physical resources”. In addition, Part 2 of the RMA requires the Council to recognise and provide for matters of national importance (s6); have particular regard to other matters (s7); and to take into account the principles of the Treaty of Waitangi (s8).
- 140) Retail activities, not ancillary to industrial activities, are not anticipated by the District Plan in the General Industrial Zone and are therefore non-complying activities. As can be seen by the objectives and policies analysis above, there is clear guidance given by the District Plan in respect of business activities, and I consider those objectives and policies are coherent and have been prepared having regard to Part 2. I therefore do not consider that a detailed evaluation of Part 2 matters as they apply to the proposal would add anything to my evaluative exercise and to my final recommendation.
- 141) However, for completeness I have assessed the proposal against Part 2 and consider that the proposal:
- a. Promotes the sustainable management of natural and physical resources, in accordance with s5 of the RMA because the proposal will involve the redevelopment of an existing site and mitigates the effects of the activity on the environment to the extent that it can.
 - b. There are no particular s6 matters aside from s6(h) relating to the management of significant risks from natural hazards. In this case, the risks are not considered to be significant as they will be mitigated by suitably designed foundations with respect to earthquake induced liquefaction and earthworks to create a flood free building platform with respect to the ponding flood hazard.
 - c. Is consistent with the other matters to have regard to under s7 of the RMA, in particular the efficient use and development of resources, amenity values and the quality of the environment.
 - d. Is not inconsistent with the principles of the Treaty of Waitangi.
- 142) Overall, I consider the proposal is consistent with the purpose of the RMA as set out in Part 2.

9. RECOMMENDATION

- 143) That the Commissioner acting under delegated authority from Kāpiti Coast District Council and pursuant to Sections 104 and 104B and with reference to 104D of the Resource Management Act 1991, grant consent to undertake earthworks on a contaminated site for the construction of

a building to be used for a retail activity (supermarket) that does not comply with the permitted activity standards for:

- Landscaping;
- Signage;
- Vehicle movements; and,
- Access

at 160 Kāpiti Road, Paraparaumu (being Lots 1 & 2 DP 63027 and Lot 3 DP 63992).

144) I have included a draft set of recommended conditions, included in **Appendix D** of this report, should the Commissioner be of a mind to grant consent.

Appendix A

Receiving Environment Resource Consents



Address	Consent	Status
196 Kapiti Road	<p>RM200042</p> <p>Residential activity (two apartments) in the Industrial Zone not complying with permitted activity standards</p>	<p>Decision issued 22/04/2020</p> <p>Building consent has been sought and is currently on hold awaiting a response to the request for further information</p>
188 Kapiti Road	<p>RM950197</p> <p>To extend the winstone glass tenancy</p>	<p>Decision issued 31/07/1995</p> <p>Consent given effect to. Most recent monitoring note is from July 1996</p>
184 Kapiti Road	<p>RM990226</p> <p>Installation of a dual faced time and temperature display sign</p>	<p>Decision issued 28/06/1999</p> <p>It appears if this sign was installed, it has been removed</p>
	<p>RM030232</p> <p>Warehouse extension and car parking and access</p>	<p>Decision issued 23/09/2003</p> <p>Consent given effect to as per monitoring notes 3/06/2005</p>
27 Birmingham Street	<p>RM990324</p> <p>Staged subdivision creating 42 residential lots, 7 industrial lots, road and 5 local purpose reserve lots</p>	<p>s.224 issued 10/08/2006</p> <p>No further actions necessary as this is a subdivision consent</p>

		RM010117 Earthworks for residential and industrial development and to create a flood detention area between Birmingham and Guildford	Decision issued 29/06/2001 Council records do not contain any monitoring notes, however given development of the site has occurred, it is reasonable to consider the consent has been given effect to
		RM970207 Subdivision creating 65 additional lots	s.224 issued 28/09/1999 No further actions necessary as this is a subdivision consent
21 Birmingham Street		RM050282 Certificate of Compliance	Decision issued 18/11/2005
		RM070178 Construction of warehouse building encroaching the height standards	Decision issued 20/06/2007 Monitoring notes from 1/07/2008 state that building has been constructed in accordance with the plans and no further monitoring is necessary
		RM070188 Certificate of Compliance for new warehouse construction	Decision issued 18/07/2007 Council records show this was constructed
11 Birmingham Street		RM950128 Subdivision creating one additional allotment	s.224 issued 20/05/1995 No further actions necessary as this is a subdivision consent
7 Arko Place		RM020270 To construct and operate warehouse that will not meet loading standards	Decision issued 16/10/2002 Monitoring notes from 6/09/2005
		RM020056 Certificate of compliance for community probation office and periodic detention centre	Decision issued 27/03/2002 Building records show this has been constructed
144 Kapiti Road		RM120041 To erect a freestanding sign	Decision issued 11/05/2012 Monitoring notes show that the sign was erected in 2006
		RM940218 Boundary adjustment	Decision issued 8/11/1994 Consent has lapsed
		RM110129	Decision issued 26/09/2011

	Construction of Commercial premises in Industrial Zone	Monitoring notes show constructed and no further monitoring required 20/10/2016
Consents relating to the Paraparaumu Airport from 2010-2022	RM210258 Earthworks for the relocation of an existing building within the site to be used as part of a regular market and construction of a building to be used in conjunction with the market and as a facility for hire	Decision issued 25/01/2022
	RM180235 Extend the existing retail activity (Mitre 10) and construct a sign that does not comply with the permitted activity standards	Decision issued 7/10/2019 Monitoring note from 20/02/2022, no building consent application received
	RM180126A To cancel condition 10 of RM180126	Decision issued 30/01/2019
	RM180126 Undertake a two-lot commercial subdivision of a site identified on the Greater Wellington Selected Land Use Register	s.224 issued 4/02/2019 No further actions necessary as this is a subdivision consent
	RM160196 To establish a new Flight Information Service Facility	Decision issued 19/10/2016 Consent given effect to. No further monitoring required as per notes 21/05/2018
	RM160155 Earthworks to allow for the extraction of sand from and backfill of cleanfill to, an area on the south-eastern side of the Kapiti Coast Airport	Decision issued 18/08/2016 No monitoring details available
	RM150208 Construction of a new home improvement store and control tower (Stages 2M/2P)	Decision issued 10/12/2015 Consent given effect to. Monitoring notes show any construction issues were resolved and no further monitoring required from 23/04/2018
	RM150088B To subdivide Lots 7 and 8 to create an additional allotment which breaches the permitted activity	s.224 issued 23/04/2018 No further actions necessary as this is a subdivision consent

	standards regarding maximum impermeable surface area to vary conditions 1, 8, 14, 15, 16 and 21 of RM150088 (as modified by RM150088A) to enable an additional allotment	
RM150088A	S.127 cancel a condition & change advice note on RM150088	Decision issued 17/11/2016 No further actions necessary as this consent relates to the subdivision approved and certified under RM150088 and RM150088B
RM150088	8 lot fee simple subdivision of Airport Zone land and road to vest	s.224 issued 23/04/2018 No further actions necessary as this is a subdivision consent
RM150022	Construction of a small-scale retail node at Friendship Place, Market Square, Kapiti Landing Business Park, Paraparaumu.	Decision issued 1/04/2015 Buildings consented have not been constructed and therefore, consent has lapsed
RM140223	2 x roof top signs at Kapiti Coast Airport Stage 1B at Kapiti Road Paraparaumu	Decision issued 5/12/2014 No monitoring information available
RM140219	Construction of a new retail building (Stage 1D) at Kapiti Landing Business Park, Kapiti Road Paraparaumu	Decision issued 20/01/2015 Consent has been given effect to
RM140214	Construction of an identifier pylon sign at Kapiti Landing Business Park, Kapiti Road, Paraparaumu	Decision issued 19/12/2014 Consent has been given effect to
RM140131A	Change of Condition 1 of land use consent RM140131	Decision issued 14/10/2016
RM140131	Proposed construction of a Service Station at Stage 2J, Kapiti Landing Business Park, Paraparaumu.	Decision issued 20/10/2014 Consent has been given effect to
RM140130	- The Construction of a building within the Airport Zone	Decision issued 18/08/2014 Consent has been given effect to – works relate to

	<ul style="list-style-type: none"> - Earthworks within 20m of a water body - Height and setback encroachments 	walkway/landscaped area along Kapiti Road
	<p>RM140060</p> <p>Extension to building 1C, additional carparking and associated works</p>	<p>Decision issued 11/06/2014</p> <p>Consent has not been given effect to and therefore has lapsed</p>
	<p>RM130155</p> <p>Construction of vehicle trade workshops at Kapiti Coast Airport, Toru Road, Paraparaumu - Stage 2K.</p>	<p>Decision issued 16/10/2013</p> <p>Consent has been given effect to</p>
	<p>RM130120</p> <p>Proposed Market Place at Kapiti Business Park, Kapiti Road Paraparaumu</p>	<p>Decision issued 13/02/2014</p> <p>Monitoring notes show consent has lapsed 20/03/2019</p>
	<p>RM130111</p> <p>Relocation of the Kapiti Aero Club at Toru Road, Paraparaumu</p>	<p>Decision issued 15/08/2013</p> <p>Consent has been given effect to as per monitoring notes 15/05/2015</p>
	<p>RM130106</p> <p>Construction of a new retail building and intersection at Kapiti Landings Business Park, Kapiti Road, Paraparaumu (Stage 1C)</p>	<p>Decision issued 19/03/2014</p> <p>Consent has not been given effect to and has lapsed</p>
	<p>RM110031</p> <p>Proposed grocery outlet at Paraparaumu Airport</p>	<p>Decision issued 8/03/2011</p> <p>Consent has been given effect to (New World)</p>
	<p>RM110009</p> <p>Establishment of temporary terminal at Paraparaumu Airport, Kapiti Road, Paraparaumu</p>	<p>Decision issued 25/02/2011</p> <p>Consent given effect to. No further monitoring required 20/10/2016</p>
	<p>RM100211</p> <p>Stage 1B development Paraparaumu Airport, Kapiti Road Paraparaumu</p>	<p>Decision issued 15/12/2010</p> <p>Consent has been given effect to, ongoing monitoring with respect to landscaping</p>
	<p>RM100127</p> <p>Proposed Temporary Passenger Terminal at Paraparaumu Airport</p>	<p>Withdrawn 21/01/2011</p>
	<p>RM100081</p>	<p>Decision issued 16/07/2010</p> <p>Consent has been given effect to</p>

	<p>Stage 1A Development of Mitre 10 Mega at Paraparaumu Airport, Kapiti Road, Paraparaumu</p>	
	<p>RM100069 Proposed earthworks and construction of Roundabout in Kapiti Road for access to Paraparaumu Airport</p>	<p>Decision issued 20/05/2010 Consent has been given effect to</p>
	<p>RM100036 Proposed Stage 1A preparatory earthworks for building platforms and accessways, northeast quadrant of Paraparaumu Airport</p>	<p>Decision issued 10/05/2010 Consent has been given effect to</p>
	<p>RM100029 Application to relocate Kapiti Cars building from current site to new area off Kapiti Rd, Paraparaumu</p>	<p>Decision issued 22/03/2010 Consent given effect to, no further monitoring required as per notes 20/10/2016</p>

Appendix B

Submissions

Submission on application concerning resource consent that is subject to public notification by consent authority

To Kāpiti Coast District Council
Submission on Resource consent application lodged by Kāpiti Retail Holdings Limited with resource consent reference number RM210151
Name of submitter Modern Merchants Limited

Introduction

- 1 This is a submission on an application from Kāpiti Retail Holdings Limited (**Applicant**) for a resource consent in respect of the construction and operation of a supermarket and two trade supplier tenancies (and ancillary works) (**Application**) at 160 Kāpiti Road, Paraparaumu (**Site**).
- 2 We are the head lessor of New World Kāpiti at 159 Kāpiti Road, Paraparaumu (**New World Kāpiti**). New World Kāpiti is located within Kāpiti Landing, a business park comprising a variety of retail activities. New World Kāpiti is on the opposite side of Kāpiti Road from the Site, and is served by the same road network.
- 3 It is acknowledged that Foodstuffs and ourselves are trade competitors for the purposes of s 308B of the Resource Management Act 1991 (**RMA**). However, we are directly affected by an effect of the activity to which the Application relates that adversely affects the environment; and our submission is limited so that it does not relate to trade competition or the effects of trade competition. On this basis, this submission does not breach the trade competition provisions of the RMA.

Scope and reasons for submission

- 4 This submission relates to the Application in its entirety.
- 5 This submission opposes the Application, to the extent that transportation effects are not appropriately avoided, remedied or mitigated and, if granted, the Application:
 - 5.1 May not promote the sustainable management of resources and therefore may not achieve the purpose and principles of the RMA;
 - 5.2 May be contrary to Part 2 and other provisions of the RMA;
 - 5.3 May not meet the reasonably foreseeable needs of future generations;
 - 5.4 May not enable social, economic and cultural wellbeing;
 - 5.5 May be contrary to the purposes and provisions of the relevant statutory planning instruments; and
 - 5.6 May not avoid, remedy or mitigate adverse environmental effects, including on the surrounding road network and New World Kāpiti.
- 6 Apart from the transportation effects noted above, this submission is otherwise neutral regarding whether the Application is granted.

7 The following reasons are given in particular, without derogating from the generality of the above reasons.

Transportation effects

8 The Application may adversely affect the operation of the road network that serves New World Kāpiti, including the operation of the Kāpiti Road / Friendship Place roundabout (**Roundabout**). Access to New World Kāpiti is also directly from the Roundabout.

9 The Application is supported by an Integrated Transportation Assessment prepared by Tim Kelly Transportation Planning Ltd dated July 2021 (**ITA**). In addition, a response to a request under s 92 of the RMA relating to transportation matters was prepared by Tim Kelly Transportation Planning Ltd dated 12 August 2021.

10 The Application and the ITA indicate that:

10.1 The primary access to the Site will be directly from the Roundabout; and

10.2 The secondary access to the Site will be at the south-east end of the Site, and will operate left-in/left-out only, with right turn entry and exit manoeuvres physically prohibited by a median barrier.

11 Arising from the Application, and in particular the ITA and the transportation s 92 response:

11.1 The traffic modelling supporting the ITA appears to consider existing traffic volumes based on traffic counts commissioned in June 2018.

11.2 It is unclear that the traffic modelling reflects the existing environment. In particular, it is unclear that the traffic modelling has considered consented, but not yet constructed, development in the vicinity (including within Kāpiti Landing)¹.

11.3 Traffic growth is mentioned in the ITA, however it is unclear whether that has been applied to the traffic modelling.

11.4 The ITA explains that the traffic modelling shows poor performance of the Roundabout without mitigation.

11.5 The ITA proposes mitigation in the form of a short (10m) additional approach lane on the Friendship Place approach to the Roundabout (**Proposed Mitigation**).

11.6 The ITA indicates that 'Space appears to be available to enable this modification [the Proposed Mitigation] with minor physical works to reposition the footpath and kerblines on the northern side of the Friendship Place approach [to the Roundabout].'

¹ Kāpiti Landing is not fully developed at this stage but it is understood that there may be some consented development within Kāpiti Landing that is not yet constructed.

- 11.7 It is unclear that the Proposed Mitigation is able to be completed entirely within the Friendship Place road reserve - the detailed plans in support of the Application do not appear to show the full extent of the Proposed Mitigation.
- 11.8 It is unclear that the Proposed Mitigation is adequate or appropriate, and in particular it is unclear that the length of the 10m additional approach lane is adequate or appropriate.
- 11.9 It is unclear that the Proposed Mitigation is adequate or appropriate if the traffic modelling needs to be updated to consider consented, but not yet constructed, development in the vicinity (including within Kāpiti Landing).
- 11.10 It is unclear whether the appropriateness of allowing right turn entry from Kāpiti Road to the secondary access to the Site has been considered by the Applicant, which may have some benefit in terms of reducing pressure on the Friendship Place roundabout (both to outbound Friendship Place vehicles and to eastbound Kāpiti Road vehicles).

Relief sought

- 12 We seek clarification of the various matters noted above, and that the Application be declined if the transportation effects are not appropriately avoided, remedied or mitigated (including through appropriate conditions of consent).
- 13 We seek to be heard in support of this submission.

Date 10 November 2021

By the submitter's solicitors and authorised agents
DLA Piper New Zealand

Signature



.....
Stephen Quinn / Waldo Randal

Electronic address for service of submitter

stephen.quinn@dlapiper.com /
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Telephone

+64 27 434 9668 / +64 21 256 1490

Postal address

C/- Stephen Quinn / Waldo Randal

DLA Piper Wellington
Level 4, 20 Customhouse Quay
Wellington 6011
PO Box 2791
Wellington 6140

Submission on application concerning resource consent that is subject to public notification by consent authority

To Kāpiti Coast District Council
Submission on Resource consent application lodged by Kāpiti Retail Holdings Limited with resource consent reference number RM210151
Name of submitter Young Supermarkets Limited

Introduction

- 1 This is a submission on an application from Kāpiti Retail Holdings Limited (**Applicant**) for a resource consent in respect of the construction and operation of a supermarket and two trade supplier tenancies (and ancillary works) (**Application**) at 160 Kāpiti Road, Paraparaumu (**Site**).
- 2 We operate New World Kāpiti at 159 Kāpiti Road, Paraparaumu (**New World Kāpiti**). New World Kāpiti is located within Kāpiti Landing, a business park comprising a variety of retail activities. New World Kāpiti is on the opposite side of Kāpiti Road from the Site, and is served by the same road network.
- 3 It is acknowledged that Foodstuffs and ourselves are trade competitors for the purposes of s 308B of the Resource Management Act 1991 (**RMA**). However, we are directly affected by an effect of the activity to which the Application relates that adversely affects the environment; and our submission is limited so that it does not relate to trade competition or the effects of trade competition. On this basis, this submission does not breach the trade competition provisions of the RMA.

Scope and reasons for submission

- 4 This submission relates to the Application in its entirety.
- 5 This submission opposes the Application, to the extent that transportation effects are not appropriately avoided, remedied or mitigated and, if granted, the Application:
 - 5.1 May not promote the sustainable management of resources and therefore may not achieve the purpose and principles of the RMA;
 - 5.2 May be contrary to Part 2 and other provisions of the RMA;
 - 5.3 May not meet the reasonably foreseeable needs of future generations;
 - 5.4 May not enable social, economic and cultural wellbeing;
 - 5.5 May be contrary to the purposes and provisions of the relevant statutory planning instruments; and
 - 5.6 May not avoid, remedy or mitigate adverse environmental effects, including on the surrounding road network and New World Kāpiti.
- 6 Apart from the transportation effects noted above, this submission is otherwise neutral regarding whether the Application is granted.

7 The following reasons are given in particular, without derogating from the generality of the above reasons.

Transportation effects

8 The Application may adversely affect the operation of the road network that serves New World Kāpiti, including the operation of the Kāpiti Road / Friendship Place roundabout (**Roundabout**). Access to New World Kāpiti is also directly from the Roundabout.

9 The Application is supported by an Integrated Transportation Assessment prepared by Tim Kelly Transportation Planning Ltd dated July 2021 (**ITA**). In addition, a response to a request under s 92 of the RMA relating to transportation matters was prepared by Tim Kelly Transportation Planning Ltd dated 12 August 2021.

10 The Application and the ITA indicate that:

10.1 The primary access to the Site will be directly from the Roundabout; and

10.2 The secondary access to the Site will be at the south-east end of the Site, and will operate left-in/left-out only, with right turn entry and exit manoeuvres physically prohibited by a median barrier.

11 Arising from the Application, and in particular the ITA and the transportation s 92 response:

11.1 The traffic modelling supporting the ITA appears to consider existing traffic volumes based on traffic counts commissioned in June 2018.

11.2 It is unclear that the traffic modelling reflects the existing environment. In particular, it is unclear that the traffic modelling has considered consented, but not yet constructed, development in the vicinity (including within Kāpiti Landing)¹.

11.3 Traffic growth is mentioned in the ITA, however it is unclear whether that has been applied to the traffic modelling.

11.4 The ITA explains that the traffic modelling shows poor performance of the Roundabout without mitigation.

11.5 The ITA proposes mitigation in the form of a short (10m) additional approach lane on the Friendship Place approach to the Roundabout (**Proposed Mitigation**).

11.6 The ITA indicates that 'Space appears to be available to enable this modification [the Proposed Mitigation] with minor physical works to reposition the footpath and kerbline on the northern side of the Friendship Place approach [to the Roundabout].'

¹ Kāpiti Landing is not fully developed at this stage but it is understood that there may be some consented development within Kāpiti Landing that is not yet constructed.

- 11.7 It is unclear that the Proposed Mitigation is able to be completed entirely within the Friendship Place road reserve - the detailed plans in support of the Application do not appear to show the full extent of the Proposed Mitigation.
- 11.8 It is unclear that the Proposed Mitigation is adequate or appropriate, and in particular it is unclear that the length of the 10m additional approach lane is adequate or appropriate.
- 11.9 It is unclear that the Proposed Mitigation is adequate or appropriate if the traffic modelling needs to be updated to consider consented, but not yet constructed, development in the vicinity (including within Kāpiti Landing).
- 11.10 It is unclear whether the appropriateness of allowing right turn entry from Kāpiti Road to the secondary access to the Site has been considered by the Applicant, which may have some benefit in terms of reducing pressure on the Friendship Place roundabout (both to outbound Friendship Place vehicles and to eastbound Kāpiti Road vehicles).

Relief sought

- 12 We seek clarification of the various matters noted above, and that the Application be declined if the transportation effects are not appropriately avoided, remedied or mitigated (including through appropriate conditions of consent).
- 13 We seek to be heard in support of this submission.

Date 10 November 2021

By the submitter's solicitors and authorised agents
DLA Piper New Zealand

Signature



.....
Stephen Quinn / Waldo Randal

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Wellington 6140

SUBMISSION ON A RESOURCE CONSENT APPLICATION THAT IS SUBJECT TO PUBLIC NOTIFICATION BY THE KAPITI COAST DISTRICT COUNCIL

Pursuant to section 96 of the Resource Management Act 1991

Application Number:	RM210151
Applicant:	Kapiti Retail Holdings Limited
Proposal:	Land use consent to undertake earthworks that do not meet the permitted activity standards for the construction of a building to be occupied by a retail activity that does not meet the permitted activity standards for the General Industrial Zone, vehicle movements, signs, access and landscaping and requires consent under the National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health
Legal Description(s):	Lots 1 & 2 DP 63027 and Lot 3 DP 63992– 160 Kapiti Road, Paraparaumu

DUE AT COUNCIL OFFICE NO LATER THAN 5:00PM ON WEDNESDAY 10 NOVEMBER 2021

This is a submission on an application from Kapiti Retail Holdings Limited to undertake earthworks that do not meet the permitted activity standards for the construction of a building to be occupied by a retail activity that does not meet the permitted activity standards for the General Industrial Zone, vehicle movements, signs, access and landscaping and requires consent under the National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health. It is located in the General Industrial zone and is a Non-Complying Activity.

Please note: This form is only a guideline. If you don't wish to use this form please make sure your submission includes all the following details (see Resource Management (Forms, Fees, and Procedure) Regulations 2003, Form 13 for official submission content requirements):

Please send your Submission to:

To:	Or:
The Chief Executive Officer	Email: submissions@kapiticoast.govt.nz
Kāpiti Coast District Council	
Private Bag 60 601	
Paraparaumu 5254	

Note: You are required to send a copy of your submission to the applicant as soon as reasonably practicable after you have served your submission on the Kāpiti Coast District Council.

Please serve a copy of your submission to Kapiti Retail Holdings Limited (the Applicant) as below:

Kapiti Retail Holdings Limited
PO Box 24463
Royal Oak
Auckland 1345

Or email: kay@formeplanning.co.nz

Attention: Kay Panther Knight

Submitter/s Details:

Title:	<input type="checkbox"/> Mr <input type="checkbox"/> Mrs <input type="checkbox"/> Miss <input type="checkbox"/> Ms <input type="checkbox"/> Dr Other:		
My/Our Full Name(s):	Templeton Kapiti Limited		
Address for service:	oleg.sennikov@templetongroup.co.nz	Post Code:	
Physical Address:	60 Toru Road, Paraparaumu	Post Code:	
Home Ph:		Work Ph:	
Home Fax:		Work Fax:	
Cell:		Email:	oleg.sennikov@templetongroup.co.nz

Note: Correspondence will be via email unless otherwise requested.

Submitter/s Position:

Trade Competition

- I am a trade competitor for the purposes of section 308B of the Resource Management Act 1991.
 I am not a trade competitor for the purposes of section 308B of the Resource Management Act 1991.

Please use a clear tick in the appropriate box below (✓) to show whether you support the application in full or in part, or oppose the application in full or in part, or are neutral.

<input type="checkbox"/> I / We support the application in full	<input type="checkbox"/> I / We support part of the application *
<input type="checkbox"/> I / We oppose the application in full	<input checked="" type="checkbox"/> I / We oppose part of the application *
<input type="checkbox"/> I / We are neutral on all aspects of the application	<input type="checkbox"/> I / We are neutral on part of the application *

* If you indicate you support, oppose or are neutral for part of the application, please clearly set out the part(s) of the application you are submitting on (including reasons) in the 'My Submission Is' section of this form below.

Reasons for Submission:

The specific parts of the application that my submission relates to are:

Give details:

Traffic effects, particularly the methodologies and conclusions in the Transport Assessment.

Please use additional pages if required.

My Submission Is:

Include further detail on whether you support, oppose or are neutral on the application or specific parts of it; and the reasons for your views:

Please see attached sheet.

Please use additional pages if required.

Decision Sought:

I / we seek the following decision from the Kāpiti Coast District Council (provide precise details including the general nature of any conditions or changes sought):

TKL seeks that further information is supplied that verifies the findings of the Transport Assessment in relation to effects on the transportation network or new information is provided that addresses the matters raised in this submission.

In the event this information is not provided, TKL seeks that the application be refused.

In the event that an accurate transportation assessment confirms that the grant of consent would adversely affect development of the Airport Zone Mixed Use Precinct in accordance with its zone provisions, TKL seeks that consent be refused.

TKL's concerns are addressed in more detail in the attached letter from Carriageway Consulting dated 9 November 2021.

TKL seeks any other relief necessary to resolve the concerns identified.

Please use additional pages if required.

Wish to Speak at Hearing:

Please indicate below whether you would like to speak at the hearing for the application (if a hearing is required). Use a clear tick in the appropriate box below (✓).

I / we do not wish to be heard and hereby make my / our submission in writing only.
(This means that you will not be advised of the date of the hearing and cannot speak at the hearing)

OR

I / we wish to be heard in respect of my / our submission (to speak at the public hearing)
(This means you can speak at the hearing. If at a later date you decide you no longer wish to speak at the hearing you can withdraw from being heard)

If others make a similar submission, I / we will consider presenting a joint case with them at the hearing.
(This is only for parties wanting to be heard)

I / we intend to call expert witness(es). Please indicate the disciplines of expected expert witnesses.
(If you do not tick this box, you can change your mind later and decide to call experts to give evidence in relation to your submission, provided you do so in time to meet any procedural direction the Hearing Panel might make) Transport Engineering

Pursuant to Section 100 of the Resource Management Act 1991, I / we request that the Council delegates its functions, powers and duties required to hear and decide the application to one or more hearings commissioners who are not members of the Kapiti Coast District Council.

If you do wish to make a request for an Independent Commissioner pursuant to Section 100, please see notes below for potential cost implications to you.

I / we are aware that I / we are required to send a copy of my / our submission to the applicant as required under section 96(6)(b) of the Resource Management Act 1991 (please tick).



10-11-2021

Signature

Date:

Signature

Date:

Please note: Signature of submitter, or person authorised to sign on their behalf is required. Signature is not required for electronic (email) submissions. If this is a joint submission by two or more individuals, each individual's signature is required.

Privacy Disclaimer

Please note: All submissions (including names and contact details) will be made publicly available at Council offices and public libraries. A summary of submissions including the name of the submitter may also be made publicly available and posted on the Kāpiti Coast District Council website. Personal information will also be used for administration relating to the subject matter of the submissions, including notifying submitters of subsequent steps and decisions. All information will be held by the Kāpiti Coast District Council, with submitters having the right to access and correct personal information.

Notes to Submitters:

- The Resource Management Act (RMA) 1991 prefers electronic methods of communication.
- The closing date for serving submissions on the consent authority is the 20th working day after the date on which public or limited notification is given. If the application is subject to limited notification, the consent authority may adopt an earlier closing date for submissions once the consent authority receives responses from all affected persons.
- If you make a request for an independent commissioner(s) under section 100A of the Resource Management Act 1991, you must do so in writing no later than 5 working days after the close of submissions and you may be liable to meet or contribute to the costs of the hearings commissioner or commissioners.
- You must serve a copy of your submission on the applicant as soon as is reasonably practicable after you have served your submission on the consent authority.
- If you are a trade competitor, your right to make a submission may be limited by the trade competition provisions in Part 11A of the Resource Management Act 1991.

Include further detail on whether you support, oppose or are neutral on the application or specific parts of it; and the reasons for your views:

Templeton Kapiti Limited (TKL) is the owner of land on the southern side of Kāpiti Road located within the Airport Zone, including the retail area known as “Kāpiti Landing”.

The Airport Zone Mixed Use Precinct (MUP) provides for a range of non-aviation commercial and other employment opportunities, including limited retail activities that are consistent with supporting the role and function of the District's centres.

The MUP is subject to standards limiting cumulative gross floor area to 102,900m² (AIRPZ-R13). In addition, any development that exceeds a cumulative gross floor area of 43,050m² requires a transport assessment to be carried out. This transport assessment must consider the impact of the cumulative development of the area on the safety and efficiency of the transport network. A further transport assessment is required for any development that exceeds 62,500m².

TKL has concerns that the additional traffic generation that will arise as a result of this application will affect the overall capacity of the transportation network in a manner that has not considered the anticipated extent of development enabled by the MUP rules and/or may adversely affect the ability of the MUP to be developed as anticipated by the MUP.

Particularly, TKL is concerned about the level of detail in the Transport Assessment submitted with the application and the conclusions about the effects on the transportation network reached as a result. The concerns with the Transport Assessment have been set out in detail in the **attached** letter from Carriageway Consulting dated 9 November 2021.

For these reasons, TKL seeks that further information is supplied that verifies the findings of the Transport Assessment in relation to effects on the transportation network, or new information is provided that addresses the matters raised in this submission.

CCL Ref: 14797-101121-sennikov

10 November 2021

Oleg Sennikov
Templeton Kapiti Limited

By e-mail only: oleg.sennikov@templeton.co.nz



A. PO Box 29623, Christchurch, 8540
P. 03 377 7010
E. office@carriageway.co.nz

Dear Oleg

RM210151: Proposed Countdown Supermarket, Paraparaumu: Review of Transportation Matters

Further to recent discussions, we understand that an independent review is sought of the proposal by Kāpiti Retail Holdings Limited for a 3,800sqm supermarket plus ancillary units at 160 Kāpiti Road, Paraparaumu. This letter sets out our review of the six documents provided to us, namely:

- RM210151: Appendix 5, Integrated Transportation Assessment by Tim Kelly Transportation Planning Limited;
- RM210151: Extract from proposal;
- RM210151: Extract from proposal addressing traffic effects;
- RM210151: Further Information Request response dated 3 August 2021;
- RM210151: Further Information Request response dated 12 August 2021; and
- Provisions of the District Plan relating to the Airport Zone Rules (dated October 2021)

We have reviewed the documents in depth but this letter has been written to be suitable for a submission and to be read by a non-expert. Consequently although we have covered all relevant matters we have sought to avoid the very high degree of detail that would typically be expected of a full peer review. Furthermore, the level of detail with regard to traffic volumes that we would usually expect to see within an Integrated Transportation Assessment (ITA) is not present in this case, which has precluded a more detailed evaluation of a number of matters.

As relevant, we have referred to the section or paragraph for ease of reference and we have focussed on the areas of the assessment where we have concerns.

Appendix 5, Integrated Transportation Assessment (Tim Kelly Transportation Planning Limited)

Section 2.3: Traffic Counts

The ITA notes that detailed turning counts were carried out by Woolworths NZ in June 2018 and by the Council in February 2018, and link volumes were counted by the Council in August 2017, 2018 and 2019, and November 2020.

Table 2.1 of the ITA only shows the link counts carried out by the Council however, with no detail presented for the turning counts of 2018, and in particular, the volume entering and exiting Friendship Place where significant delays are forecast. We discuss this omission subsequently.



Section 2.4: Traffic Volumes

The traffic volumes during the Saturday peak hour are noted as being “*slightly higher*” than in the weekday evening peak hour, with the weekday evening period and during the late morning period on Saturdays being the periods used for assessment. We discuss this subsequently.

Section 2.4: Traffic Growth

The traffic growth factors have been found from evaluating the historic growth rates. While this is an appropriate approach, it shows that while the weekday peak hour, and the daily weekday and daily Saturday volumes have increased by around 1.5%, the Saturday peak hour volume has reduced by 1.5%. When the data is evaluated further, this appears to be largely due to a reduction of 10.2% in the westbound traffic volume in the Saturday peak hour.

Such a reduction is very large, and in our view it is plausible that it arises because the 2017, 2018 and 2019 counts were carried out in the month of August, but the 2020 count was carried out in November. The Waka Kotahi factors for scaling the traffic generation of developments (set out in Table B.2 of Research Report 453 ‘*Trips and Parking Related to Land Use*’) show that with all other factors held constant, a traffic count carried out in November would be expected to be 8.3% lower than a count carried out in August. In other words, the ITA has not compared ‘like with like’ but rather, the difference may arise because of the timing of the surveys.

The ITA sets out that the difference may be due to ‘peak spreading’ whereby drivers are adjusting their travel time to avoid the busiest periods. If peak spreading is assumed to occur, then the traffic volumes set out in Table 2.1 show that it occurred at a threshold of 913 vehicles per hour (as in the next year, the volume had reduced to 820 vehicles per hour). Therefore this is the ‘ballpark’ volume at which peak spreading should occur on this section of the road. However such peak spreading did not occur in the previous year, when the traffic volumes were similar. Peak spreading also does not occur in the westbound direction where volumes of more than 950 vehicles were observed. This is not the outcome that would be expected if peak spreading explained the difference.

On this basis, the differences are not in our view explained by peak spreading, but are more likely to be due to the traffic counts being carried out in different months. We therefore consider that the negative growth rate seen in the Saturday peak hour is not reliable.

This matter also affects the calculation of traffic growth during the weekday – again, the use of a different month where traffic volumes are expected to be lower means that the annual growth rate calculated is likely to be lower than is currently occurring.

Section 4.2: Assessment Methodology: Traffic Model

The approach set out, of considering the situation with a base year (in this case 2018) and a future design year (in this case 2026) is appropriate but this necessarily involves factoring the 2018 traffic volume data. In order to do this, for the Saturday peak hour, the negative growth rate calculated above has been applied (as confirmed in the first paragraph of Annexure C, section 2). The specific turning volumes used are not shown in the ITA, but Table 4.1 shows that the total traffic volume used in 2026 on Saturday was 2,018 vehicles compared to 2,293 vehicles in 2018. This is a 12% reduction, which corresponds to 8 years of -1.5% growth. As noted above, we do not consider that this negative rate is reliable.

Furthermore, this approach is inconsistent in our view. If peak spreading starts to occur around 913 vehicles per hour (the volume that Table 2.1 shows occurred before the volume started to reduce), this does not mean that traffic volumes continue to decline in every future year. Rather, volumes



should remain fairly static around this threshold because it represents the upper limit of capacity. They would not continue to decrease into the future.

Applying a 1.5% reduction to the traffic flows each year is therefore not the appropriate approach in our view. Rather, it will lead to the volumes on the road network during the Saturday peak hour, noted in the ITA as being the highest peak hour, being underestimated.

As noted above the growth rate calculated for weekdays is also likely to be lower than the current rate, as a result of this methodology.

Section 4.2: Assessment Methodology: Assessment Periods and Scenarios

The approach of considering a design year and making allowance for any “*consented development*” in the area is valid in our view, but there is no indication in the ITA of what developments have been taken into account nor of their effects on the traffic flows. However the response to the Further Information Request sets out that in practice, no consented development has been allowed for.

There is also no commentary provided regarding whether permitted development in the area, that is, development which could occur but for which no consents are needed, has been taken into account. With that in mind, we have reviewed the District Plan provisions for the Airport Zone and note that:

- Kāpiti Coast Airport is a significant transport infrastructure node (AIRPZ-P2); and
- Business activities will be undertaken with regard to connectivity and access within and to the Working Zones and opportunities for transport choice and efficiency will be maximised (AIRPZ-P4); and
- Development thresholds are set out with the Airport Mixed Use Precinct only requiring a Transportation Assessment when a cumulative gross floor area of 43,050sqm is exceeded.

We note that the Airport Mixed Use Precinct is served by Friendship Place, with access onto Kāpiti Road at the same roundabout as proposed by the supermarket. Since the District Plan is newly operative, we consider that this strongly suggests an assumption of growth in activities in this Precinct, which can be expected to increase traffic flows through the roundabout. However there is no indication in the ITA that this has been taken into account.

In respect of this, and the modelling approach, we note that the Further Information Request queries why the Council’s transportation model was not used for the analysis. Use of a complex transportation model of this nature typically enables permitted development to be taken into account (as it is included in the model) and also enables a comparative assessment of traffic growth. Even if ultimately the Council’s model was not used for the analysis, we consider that it would have been helpful for the volumes and growth output to have been used for benchmarking the analysis presented, to show that it is not inconsistent. It would also have been helpful to demonstrate that the rationale for future negative traffic growth is supported.

Section 4.3: External Vehicle Access Points

The ITA notes that as a result of the proposed supermarket, the traffic passing through the Kāpiti Road / Friendship Place roundabout would increase by 14-15%. It is also noted that this increase in the weekday evening peak period leads to a change from Level of Service B or C, to F. This is due to vehicles being unable to exit from Friendship Place.

There is insufficient information presented in the ITA to enable an independent check of this because as noted earlier, the traffic flows used in the analysis are not included. In particular, the turning volumes to/from Friendship Place are not included in the report, despite this being the



roundabout approach that experiences the greatest change in delay. This also means that we are unable to evaluate how many drivers exiting the site are assumed to turn towards the southeast via the secondary access, compared to how many will pass through the Kāpiti Road / Friendship Place roundabout. This may affect the modelling results further.

However the delays are included in Table 4.1, and this shows that for traffic on Friendship Place, delays increase from around 26 seconds per vehicle to around 130 seconds per vehicle during the weekday peak hour. In our view, such an increase is substantial.

The ITA notes that on Saturday, the existing roundabout would operate with acceptable levels of queuing and delays. We note that as a negative growth rate has been applied, then this is not unsurprising – the supermarket increases traffic volumes but the negative growth rate reduces them. We note that the increase in traffic volumes is almost completely offset through the approach used in the ITA¹.

In passing, the ITA notes that the secondary access towards the south would operate as left-in/left-out only. However there is no indication given as to how this will be implemented. We would expect that a raised median will be required within the flush median to prevent drivers from attempting to turn right.

Section 4.5: Kāpiti Road / Friendship Place Roundabout – Mitigation

The mitigation suggested for the roundabout is the provision of an additional traffic lane on Friendship Place. On our assessment of the Council's GIS information though, the legal width of this part of Friendship Place is only around 13m and it appears unlikely that a second traffic lane could be added and achieve a roundabout geometry that meets appropriate guides/standards without requiring land that is presently outside the legal road corridor. We note that no design has been provided for this (we would not expect a detailed design, but a sketch showing a viable 'proof of concept' would typically be included within an ITA).

Section 4.8: Walking and Cycling

The use of cycling for smaller shopping trips is common, and therefore we expect that there would be an increase in this form of travel, as well as walking trips. While a qualitative assessment has been carried out of the effects of this, we note that the increase in traffic volumes will potentially give rise to a reduced level of service for pedestrians walking north-south along Kāpiti Road and there may also be road safety effects at the roundabout for cyclists. Both of these could be quantified to fully evaluate their effects, but this has not been done.

4.11: Servicing

The ITA notes that servicing will occur with vehicles travelling clockwise around the main building. For this, vehicles will be required to turn around the Kāpiti Road / Friendship Place roundabout, and the ITA refers to swept paths shown on Figure 3.1. This is at a small scale but we note that:

- The vehicle shown is a truck and trailer. However typically the most onerous vehicle type which is associated with a supermarket is a semitrailer or a B-train.
- The swept paths shown appear to have omitted the required 0.5m clearance around the vehicle. In some locations the vehicle touches the kerbs, which in practice means their wheels will scrape against the kerbface or there will be a minor amount of over-running.

¹ Table 4.1 shows that in 2026 plus the supermarket plus the application of negative traffic growth, the total traffic volume is just 0.5% greater than was observed in 2018.

Without adopting the required 0.5m clearance from the vehicle and allowing for the appropriate type of vehicle, we cannot confirm that service vehicles will be able to easily enter and exit the site.

We also note that the swept path for the exiting vehicle is not fully shown and therefore it has not been demonstrated that the vehicle can undertake this movement without travelling into the opposing traffic lane.

Section 6.3: KCDP Part 2 District Wide Matters / Transport: Rules

In reviewing the compliance with the Rules, we note that the ITA states “the primary access will utilise the existing fourth arm of the Kāpiti Road / Friendship Place roundabout”. However this reference to ‘existing’ is not strictly true. The existing fourth arm currently meets the roundabout at 90-degrees (as shown in yellow below). However Figure 3.1 of the ITA shows that the arm is to be realigned (as shown in red below):

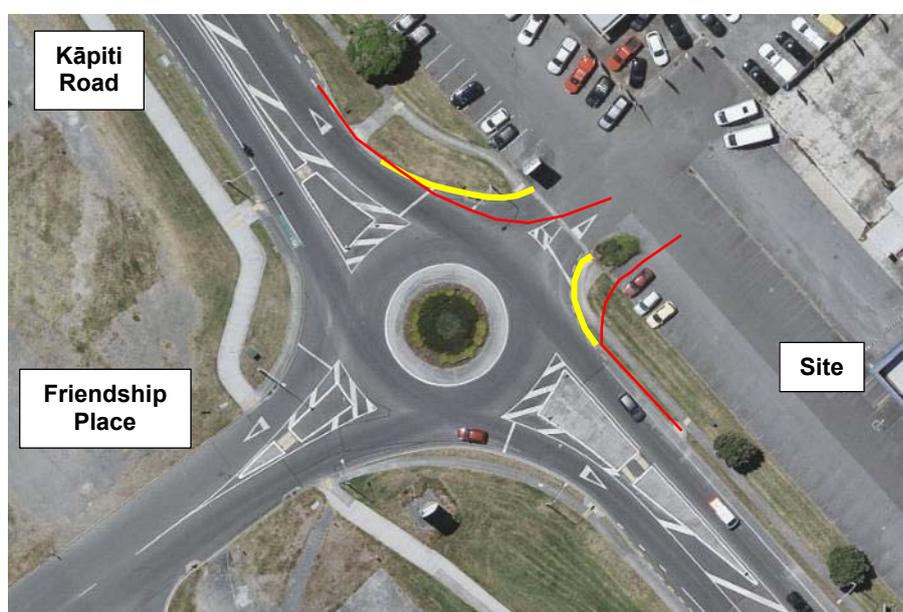


Figure 1: Existing Kāpiti Road / Friendship Place Roundabout (Fourth Arm Highlighted)

For completeness, Figure 3.1 of the ITA is at low resolution and therefore there is a margin of error in Figure 1. However based on Figure 3.1, the realignment of the fourth arm is in the order of 20 degrees. One outcome of this is that there is minimal deflection provided for traffic exiting the supermarket, that is, drivers do not have to deviate their route but can drive straight ahead onto the circulating carriageway. Reduced deflection is often associated with elevated road safety risk. The revision to the fourth arm also means that any vehicle exiting the site to turn south will have to turn through a greater angle. While this often does not create difficulties for drivers of smaller vehicles, in the event that larger vehicles were to undertake the movement, they may not be able to avoid over-running the kerb.

While we appreciate that Figure 3.1 is not a detailed design, we consider that the ITA does not show that a roundabout design meeting appropriate standards/guides can be achieved.



Summary

Having reviewed the ITA, we consider that:

- The use of negative growth, that is, a diminishing volume of traffic, is very unusual in our view. This will tend to mean that the future year traffic volumes are lower than could be expected though ambient traffic growth. The rationale provided (peak spreading) does not correspond well to the results, since it means that peak spreading occurs at a particular threshold in one direction on the road while in the other direction it does not occur even though volumes are greater. We consider that a more likely explanation is that the traffic count was carried out in a different month. Even if peak spreading was occurring, the application of ongoing negative growth into the future is not supported – rather, under peak spreading, volumes should remain the same.
- The same approach (of using different months for the calculation of traffic growth) is likely to have also resulted in the weekday peak hour growth rates being calculated to be lower than is actually the case.
- The analysis does not appear to account for consented development, nor for traffic that could be generated as of right and served from Friendship Place.
- Taking the above matters into account, we consider that the projected 2026 traffic volumes on the network are underestimated. However because the report does not include current volumes on Friendship Place, a revised assessment using more appropriately scaled volumes cannot be undertaken. Moreover, because negative growth has been applied to the Saturday late-morning period (which the ITA identifies as the period when in 2018 the volumes were greatest), the effect of the supermarket at this time is shown to be negligible, because the extra traffic generated is almost completely offset by the assumed reduced traffic volumes on the network.
- Thus the most critical time periods (from a transportation perspective) have been tested using volumes that in our view are artificially low.
- No allowance has been made for permitted development that could occur within the Airport Mixed Use Precinct, and which would be served by Friendship Place. Permitted development will increase traffic flows even further from the 2026 'baseline' volumes of the ITA.
- The level of detail in the report does not enable an independent evaluation to be carried out of the split of vehicles using the secondary access (to the south of the roundabout) compared to passing through the roundabout. This could increase traffic flows at the roundabout even further and hence increase the queues and delays forecast.
- The modelling shows that the level of service at the Kāpiti Road / Friendship Place roundabout would decrease such that delays of more than two minutes per vehicle for traffic exiting Friendship Place would arise. The ITA responds to this by proposing a second approach lane for Friendship Place, but this is not demonstrated to be formed within the legal road reserve.
- Another revision made at the roundabout is that the site access is reorientated towards the south, meaning that it would meet the roundabout at approximately 20 degrees from the perpendicular (rather than the perpendicular angle currently in place). This can lead to difficulties in achieving appropriate roundabout geometries, and the ITA does not include sufficient detail to show how a suitable layout can be provided.
- The swept paths provided do not allow for a 0.5m clearance around a service vehicle, and may not show the most onerous vehicle. The swept path for an exiting service vehicle does not demonstrate that the movement can be carried out without over-running the kerb (or running through the opposing traffic lane of Kāpiti Road) because it stops at the site boundary.



- A quantitative assessment has not been included of the effects of the proposal on walking and cycling.

Overall, one of the underlying concerns we have with the report is the absence of information regarding the traffic flows. Within an ITA we would normally expect to see a presentation of the data collected through traffic surveys, the traffic volumes expected in a future year without the development in place, and the traffic flows expected with the development in place. These are commonly (but not always) presented using 'stick diagrams' which show the turning volumes, but irrespective, the inclusion of the turning volumes means that the core calculations of the ITA can be checked. Best practice in respect of ITA production remains as set out in Waka Kotahi Research Report 422 'Integrated Transport Assessment Guidelines' which confirms that an ITA should include turning counts at critical intersections.

In this case though, no information has been presented regarding the current or future traffic volumes at the most critical location of the Kāpiti Road / Friendship Place roundabout particularly with regard to vehicles turning to and from Friendship Place. In turn, this means that there can be no independent check on the volumes used, nor any check on the modelling outcomes, nor any sensitivity testing of (say) applying a different traffic growth factor or assessing whether additional development within the Airport Mixed Use Precinct has a material effect on the results.

Conclusions

Based on the methodology set out in the ITA, we consider that the prevailing future traffic flows adjacent to the site have been underestimated. As such, the analysis of the Kāpiti Road / Friendship Place roundabout presents queues and delays that are better than will arise in practice. However we are unable (due to the lack of traffic volumes in the ITA) to undertake our own modelling using different parameters. As a result, we cannot confirm that the mitigation proposed at the roundabout will be appropriate.

We also have concerns that the proposed roundabout layout presented does not achieve appropriate design standards/guides. While we appreciate that the layout presented is a sketch rather than a detailed design, it includes elements that suggest a non-complying design may arise. As such, we cannot confirm that the proposed site access (which angles the arm through 20 degrees from the perpendicular), nor the deflection for traffic, nor the proposed additional approach lane on Friendship Place, are able to be achieved.

Overall then, we do not consider that the ITA demonstrates that the proposed supermarket can be accommodated on the adjacent roading network without adverse efficiency and/or safety effects arising.

Please do not hesitate to contact me if you require anything further or clarification of any issues.

Kind regards
Carriageway Consulting Limited

Andy Carr
Traffic Engineer | Director

Mobile 027 561 1967
Email andy.carr@carriageway.co.nz

Appendix C

Statement of Evidence of Neil Trotter

KAPITI COAST DISTRICT COUNCIL

UNDER the Resource
Management Act 1991

IN THE MATTER OF Consent application
RM210151 – Proposed
Supermarket, 160
Kāpiti Road,
Paraparaumu

**STATEMENT OF EVIDENCE OF NEIL SCOTT TROTTER
ON BEHALF OF THE KAPITI COAST DISTRICT COUNCIL**

DATE: 10 February 2022

INTRODUCTION

- 1.1 My full name is Neil Scott Trotter. I am the Transport Safety Leader at the Kāpiti Coast District Council (District Council). I have held this position since November 2020. I hold a Higher National Certificate in Civil Engineering Studies from Reading College of Technology in the UK. I have a total of 34 years' experience in the field of Civil Engineering, of which I have spent the last 23 years specialising in Transport Planning. I am also a Chartered Member of the Engineering New Zealand (CMEngNZ) and a Chartered Professional Engineer (CPEng).
- 1.2 As part of my current role in the Infrastructure Team, I provide transport advice to the Planning team(s) on resource consent applications and the Operative District Plan 2021 (ODP). This includes general advice about interpretation of the District's rules and standards and specific advice on transport effects and on the provision of transportation infrastructure.
- 1.3 I have read the Code of Conduct for Expert Witnesses contained in the Environment Court Practice Note 2014 and have complied with it in preparing this evidence. I confirm that the issues addressed in this evidence are within my area of expertise and I have not omitted material facts known to me that might alter or detract from my evidence.

2. OUTLINE OF EVIDENCE

- 2.1 My evidence limited to matters relating to the existing and proposed transport environment and providing an assessment of the proposed Countdown Supermarket against transport matters in the ODP. My evidence is organised under the following headings:
 - (a) Current Transport Environment
 - (b) Future Transport Environment
 - (c) Proposed Development
 - (d) Transport Assessment
 - (e) Impact on Friendship Place/Kāpiti Road Roundabout
 - (f) Wider Network Impacts
 - (g) Kāpiti Road Access
 - (h) Car Parking and Circulation
 - (i) Walking and Cycling
 - (j) Servicing
 - (k) Conclusion
 - (l) Recommendation

3. CURRENT TRANSPORT ENVIRONMENT

- 3.1 Kāpiti Road has a posted 50 km/hr speed limit and is classified in the ODP as a major community collector. The road provides access to the Paraparaumu

Railway Station and Transport Hub, SH1 Expressway, Paraparaumu Town Centre, the airport, the wider Paraparaumu area including Paraparaumu Beach, and tourist attractions such as Kapiti Island.

- 3.2 To the east of the site, Kāpiti Road is a two-lane road separated with a wide flush median punctuated with right turning bays, the road has a footpath on the north side of the road (site frontage) and a shared path (pedestrians and cyclists) on the southern side of the road. The road is also provided with street lighting.
- 3.3 To the west of the site, Kāpiti Road has similar formation of that to the east as a two-lane road with a wide flush median incorporating right turn bays. As with the eastern section of road, there is street lighting and a footpath on the north and a shared path to the south. Main access to the site is via a single lane roundabout serving the industrial/commercial properties to the north and the commercial properties (Kapiti Landing) to the south. Secondary access via a left in/left out arrangement is proposed by utilising the sites existing kerbed driveway access to the east of the roundabout.
- 3.4 Kāpiti Road is the busiest local road in the Kapiti District; in December 2021 on the section of road adjacent to the proposed supermarket and east of the Friendship Place/Kāpiti Road roundabout there were over 17,000 vehicles per day (vpd). Kāpiti Road does not have obvious weekday am and pm peak periods in that the traffic volumes increase from around 8am and stay relatively high until around 6pm when they being to drop. The busiest hour for two-way traffic on the section road fronting the site is 11am-12pm carrying around 1,400 vehicles per hour (vph).
- 3.5 The two-way weekday traffic flows (five-day average) on this section of road, during the typical travel to work periods of 8-9am and 5-6pm are 1,199 and 1,246 vph respectively.
- 3.6 From the traffic count undertaken on 18 December 2021, weekend daily traffic flows are around 17,500 vpd and 14,400 vpd on a Saturday and Sunday respectively. The Saturday peak traffic hour is 10-11am with over 1,600 vph, Sunday peak traffic hour is 1-2pm with over 1,400 vph. The Saturday peak hour is of note, being higher than the busiest peak hour during the weekday period.

4. FUTURE TRANSPORT ENVIRONMENT

- 4.1 The roading network in and around Kāpiti Road is expected to change in the future, including a new link between Arawhata Road and Ihakara Street (expected around 2023/2024), localised widening of Kāpiti Road, the potential for further development at the airport and a new link from Ihakara Street to the airport which will run parallel to Kāpiti Road. These, schemes, as well as improved connectivity to the Kāpiti Coast resulting from the Expressways and Transmission Gully, means that traffic levels are expected to change on Kāpiti Road in the future. The predicted changes 100m west of the Kāpiti Road / Te Roto Drive intersection are shown in the table below and have been obtained from the 2018 Kapiti Traffic Model.

Year	Motor vehicles (vpd)		
	Eastbound	Westbound	Total (two way)
2018	9,762	9,657	19,419
2026	8,710	8,325	17,035
2036	7,951	7,360	15, 311

4.2 Traffic flows from the 2018 Kapiti Traffic Model on Kāpiti Road, between Regent Drive and Langdale Avenue are as follows:

Year	Motor vehicles (vpd)		
	Eastbound	Westbound	Total (two way)
2018	7,377	8,638	16,015
2026	8,019	8,741	16,760
2036	7,169	7,804	14,973

4.3 While these predictions show a decrease in traffic on Kāpiti Road in the vicinity of the proposed supermarket, the traffic model also shows:

- Kāpiti Road to the east of the Te Roto Drive intersection carries around 26,000 vpd in 2018 and around 32,000 vpd in 2036, and
- an increase in traffic on Te Roto Drive from around 9,541 vpd in 2018 to 13,202 vpd in 2036

4.4 Pedestrian and cyclist changes are more difficult to predict, however, monitoring is showing that cycle / pedestrian activity is steadily increasing. This upward trend is likely to continue to increase over time as the shared path along Kāpiti Road is completed to the east of the Expressway and as a result increasing housing and commercial development and improved transport connectivity.

5. PROPOSED DEVELOPMENT

5.1 The Applicant proposes to construct and operate a Countdown supermarket on the subject site, comprising the following key elements:

- 3,800m² supermarket building, including 200m² of office and 200m² for online (Pick up) activities;
- 211 parking spaces on-site, including 6 parent parks, 6 accessible parks and 6 designated Pick-up parks for customers collecting online orders;
- All vehicular access from Kāpiti Road via two crossings, one via a modified roundabout intersection with Friendship Place;
- Re-use and modification of an existing access for left turn manoeuvres only to/from Kāpiti Road;

- (e) Rear service vehicle access for deliveries.

6. TRANSPORT ASSESSMENT

6.1 Triggers for the consent for the supermarket, in relation to transport were as follows:

- (a) Vehicle movements exceeding permitted activity standards for Kāpiti Road (100 vehicles movements per day) – restricted discretionary activity pursuant to Rule TR-R10.
- (b) Activities not meeting permitted activity standards in relation to access and transport, including access design and landscaping at adjacent boundaries – discretionary activity pursuant to Rule TR-R11.

6.2 As part of the assessment of effects relating to the District Plan rules set out above, an Integrated Transport Assessment (ITA) was produced by Tim Kelly Transportation Planning Limited and submitted in support of the consent for the supermarket. The ITA identified and assessed the following potential effects:

- (a) External vehicle access points - safe and efficient operation;
- (b) Additional vehicular activity on the wider area road network;
- (c) Internal circulation and parking arrangements – safe and efficient operation;
- (d) Off-street parking provision – supply of spaces relative to likely demand;
- (e) Walking and cycling – impacts upon existing movements, promotion and accommodation of additional activity;
- (f) Public transportation accessibility;
- (g) Disabled visitors and staff – adequate provision of parking and access arrangements;
- (h) Servicing – provision of appropriate (un)loading facilities and manoeuvring areas.

6.3 The ITA covered all the issues listed above, with the focus being the traffic effects on the wider road network and the operational performance of the Friendship Place/Kāpiti Road roundabout.

6.4 To assess the traffic effects a SIDRA network traffic model was developed by the Applicant. The model simulated conditions on Kāpiti Road for 11 intersections between Langdale Avenue and Arawhata Road.

6.5 The model periods chosen were the weekday hour of 4.15 to 5.15pm and the Saturday hour of 11:30am to 12:30pm. A base year of 2018 (using pre-covid counts) was used and the forecast year of 2026 (assumed as the design year – e.g., a few years after opening of the store).

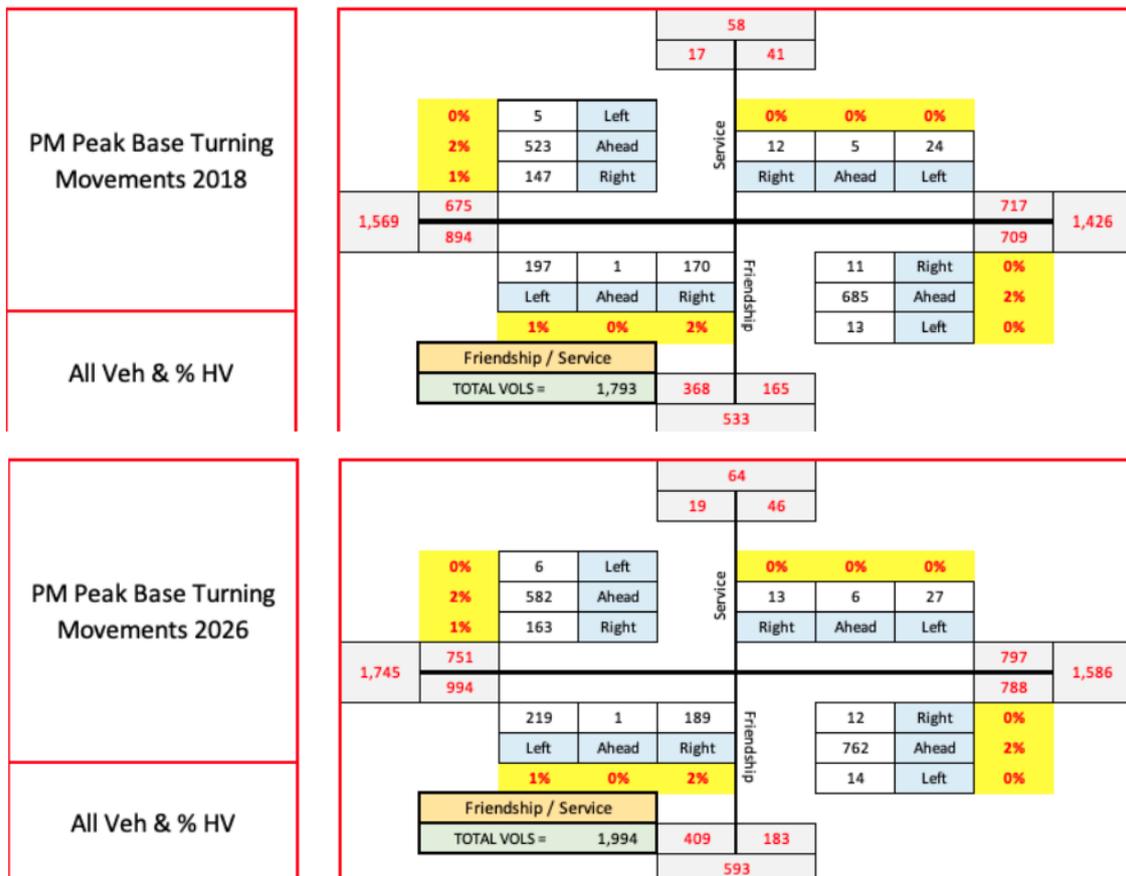
6.6 Due to the nature of shopping trips related to food shopping there is a high dependency of motorised vehicle travel to transport goods purchased at a supermarket. However, there is an acknowledgement that a limited number of

trips will be made by non-car models given the location of the proposed supermarket being reasonably close to residential areas and other commercial properties.

- 6.7 Furthermore, a proportion of vehicle trips will not be new to the road network, as with most retail land uses, and a proportion of trips will be 'pass by' trips. e.g., traffic that is already on the network which diverts to the supermarket for a specific visit and then resumes the journey on the same route.
- 6.8 In considering these two effects the ITA has reduced the trip rates by 35%, this comprises 5% to allow for non-car mode trips and 30% for pass by trips. There are other trip reducing factors that may also apply to the supermarket but due to the uncertainty around these the ITA has not made further adjustments to trip rates. Without the possible further adjustments to trip rates, the ITA concludes that the trip rates used are therefore conservative, I agree with this conclusion.

7. IMPACT ON FRIENDSHIP PLACE/KĀPITI ROAD ROUNDABOUT

7.1 I do not propose to repeat the detailed traffic assessment in my evidence; however, the following diagrams summarise the assessed peak hours of operation for the supermarket. These peak hours are 4.15pm to 5:15pm on a weekday and 11:30am to 12:30pm on a Saturday. The base traffic movements for the years 2018 and 2026 and 2026 movements resulting from the supermarket combined with the existing and forecast background traffic flows:



PM Peak Option Turning Movements 2026	422											
	233					189						
All Veh & % HV	0%			108	Left	0%			0%			
	2%			554	Ahead	116			26	47		
	1%			163	Right	Right			Ahead	Left		
	1,887		826								790	
			1,061								843	
					219	21	189					
					Left	Ahead	Right				103	
					1%			0%			2%	
					Friendship / Service						725	
					TOTAL VOLS = 2,287			429			204	
											633	

SAT Peak Option Turning Movements 2026	467											
	266					201						
All Veh & % HV	0%			123	Left	0%			0%			
	2%			654	Ahead	117			24	60		
	0%			168	Right	Right			Ahead	Left		
	1,922		944								988	
			977								767	
					236	27	275					
					Left	Ahead	Right				117	
					0%			0%			1%	
					Friendship / Service						625	
					TOTAL VOLS = 2,451			538			218	
											756	

- 7.2 Council agrees with the Applicant in the application of trip rates, the time periods assessed, the geographic extent of the model.
- 7.3 The application of trip rates to the development result in the expected generation of 400 to 430 vehicular movements in the assessed peak hours. The effect of additional traffic on the roundabout in the **weekday pm peak** period will be an increased delay and reduced level of service on the Friendship Place approach arm of the roundabout to unacceptable levels, reducing from Level of Service (LOS) B to LOS F.
- 7.4 The applicant is proposing to mitigate this issue by widening and lengthening the Friendship Place approach arm of the roundabout to create a left turn lane that will enable traffic to move freely out onto Kāpiti Road. The effect of the road widening has been retested in the Sidra model by the applicant and this shows that with the widening the Level of Service (LOS) improves from LOS F to LOS B.
- 7.5 I accept that this mitigation can be achieved within road reserve and will provide an adequate LOS for the Friendship Place arm of the roundabout in the forecast year of 2026 with the supermarket in operation.
- 7.6 The Section 92 process raised questions around the use of traffic data and the applicability of that data and how growth had been applied to the forecast years. The main concerns were related to the use of growth rates derived from traffic counts and negative growth calculated for the Saturday 2026 forecast year based on the trend growth rate from 2017-2020 traffic counts.

- 7.7 I agree that the growth rate of 1.4% is acceptable for the weekday pm peak period.
- 7.8 The Applicant has applied a negative growth factor of -1.5% per annum for the year 2026. This approach to calculating growth for Saturday traffic flows is in theory correct, however it is concerning to me that the assessment is based on a future year with reduced traffic flows. Typically, traffic continues to grow and in quite rare circumstances will it reduce significantly.
- 7.9 In discussions with the Applicant, I requested that the future year of 2026 be tested for the **Saturday peak period** assuming no reduction in growth to test a forecast year with zero growth to see what effect this would have on roundabout performance.
- 7.10 The test was carried which showed a LOS F at the Kāpiti Road (north-west arm) of the roundabout. In order provide mitigation for the poor LOS, the Applicant then tested the roundabout again with a widened arm to create a left turn lane and a separate right/through lane on the north-west arm (Kapiti Road arm) of the roundabout. The results of this test showed the arm would operate at LOS A/B. The model output also showed acceptable LOS results for the other arms of the roundabout.
- 7.11 I accept this result and the mitigation that has been proposed and assessed.
- 7.12 It is my opinion that the Applicants offer of a condition to monitor delays on a Saturday and if adverse effects arise, implement the mitigation works to create a left turn lane on the north-west arm, is an appropriate way to deal with this potential issue.

8. WIDER NETWORK IMPACTS

- 8.1 The ITA has assessed the wider network impacts by examining travel times along the Kāpiti Road corridor for the weekday pm and Saturday peak periods using the outputs from the SIDRA network model. This shows that travel times increase by 3.3 seconds per vehicle during the weekday PM peak and 10-12 seconds in the Saturday peak. The LOS changes are minimal (a LOS change of one level) at the intersections along the corridor except the Friendship Place/Kāpiti Road roundabout and this has been addressed by the Applicant. This indicates to me that there are limited effects on the wider network.

9. KĀPITI ROAD ACCESS

- 9.1 Along with the existing roundabout access to the site, the Applicant is also proposing to re-use the existing access located east of the roundabout on Kāpiti Road. The access is current configured to cater for all traffic movements (left and right). The applicant is proposing to modify the access to create a left in/left out access.
- 9.2 The modifications to the access include providing an island in the centre of the access to help control traffic movements and provide a refuge island for pedestrians crossing the access, along with a raised island in the central median on Kāpiti Road. The raised island on Kāpiti Road is proposed to physically prevent right turning movements to and from the access. This is considered a suitable mitigation for safety and capacity reasons to minimise the delay and conflicts

associated with the potential for increased right turning movements to and from the proposed supermarket.

- 9.3 The mitigation proposed is acceptable from a transport safety and delay/capacity point of view.

10. CAR PARKING AND CIRCULATION

- 10.1 The Applicant is proposing to provide a total of 211 car parking spaces in accordance with dimensions that are compliant with the ODP and AS/NZS 2890.1¹. The Applicant has assessed a suitable ratio of car parking for the uses on the site and concluded that the number of spaces proposed is also suitable to cater for the proposed development. I agree with this assessment and that the car parking layout proposed is compliant with the District Plan.
- 10.2 The Applicant is also proposing accessible parking also in accordance with design standards² and the number required for the land use in accordance with the district plan. Twelve accessible parking spaces will be provided in total, six for disabled users and six for parents with young children.
- 10.3 In addition, the Applicant is also providing ten spaces for electronic vehicles.

11. WALKING AND CYCLING

- 11.1 The Applicant has considered walking facilities on Kāpiti Road and made provision for pedestrians to cross access points and enter the store on foot with a footpath covered walkway and raised crossing points through the car park from Kāpiti Road. I consider that these facilities are appropriate to serve the development and can be used safely by pedestrians. The Applicant is proposing to provide secure covered on-site cycle parking 16 bicycles, this is more than the requirement of the District Plan and is therefore acceptable.
- 11.2 The Applicant is also proposing a Travel Plan which, if implemented, would assist and support travel to the site by non-car modes. Council is fully supportive of this initiative and a suitably worded consent condition to formalise the travel plan requirement is recommended.

12. SERVICING

- 12.1 Servicing of the site will be via the roundabout on Kāpiti Road, with large service vehicles entering the site and travelling around rear of the supermarket to the service area to the east side of the building. All servicing is undertaken in dedicated areas which are outside customer access areas. All large vehicles will exit the site via the left in/left out access on Kāpiti Road. Smaller vehicles such as courier vans will use the car park for deliveries and exit and enter the site via all access points.
- 12.2 The Applicant has carried out swept path analysis of the largest expected delivery vehicles (semi-trailer) entering and exiting the site via the route described above which demonstrates that the geometric layout of the proposed access points

¹ Parking Facilities. Part 1: Off Street Car Parking. Standard AS/NZS 2890.1:2004. Standards New Zealand, 2004.

² Parking Facilities. Part 6: Off Street Parking for People with Disabilities. Standard AS/NZS 2890.6:2009. Standards New Zealand, 2009.

to/from the site and the internal service roads and areas can accommodate the expected vehicles. I agree with this assessment.

13. SUBMISSIONS

13.1 I have studied the various submissions that have been received in response to this application and I offer the following comments on specific transportation issues that have been raised.

13.2 Arising from the Application, and the ITA and the transportation s92 response (Young Supermarkets and Modern Merchants Submissions):

(a) The traffic modelling supporting the ITA appears to consider existing traffic volumes based on traffic counts commissioned in June 2018.

My reply: Given the variations in traffic flow experienced in 2019, 2020 and 2021 due to Covid19, the application of 2018 traffic data as a base year was considered appropriate in my opinion.

(b) It is unclear that the traffic modelling reflects the existing environment. In particular, it is unclear that the traffic modelling has considered consented, but not yet constructed, development in the vicinity (including within Kapiti Landing)

My reply: The modelling includes the consented developments (only Mitre 10 Mega extension consent) at Kapiti Landing and the effects on the roundabout at Friendship Place/Kāpiti Road have been assessed.

(c) Traffic growth is mentioned in the ITA, however it is unclear whether that has been applied to the traffic modelling.

My reply: The questions around growth have been addressed, refer to Section 7.6 to 7.10 of my evidence.

(d) The ITA explains that the traffic modelling shows poor performance of the Roundabout without mitigation.

My reply: This is correct.

(e) The ITA proposes mitigation in the form of a short (10m) additional approach lane on the Friendship Place approach to the Roundabout (Proposed Mitigation).

My reply: This is correct.

(f) The ITA indicates that 'Space appears to be available to enable this modification [the Proposed Mitigation] with minor physical works to reposition the footpath and kerblines on the northern side of the Friendship Place approach [to the Roundabout]. Kapiti Landing is not fully developed at this stage, but it is understood that there may be some consented development within Kapiti Landing that is not yet constructed.

My reply: The Applicant has provided sufficiently detailed concept drawings that show that the works can be accommodated within road reserve. The modelling includes the consented developments (only Mitre 10 Mega extension consent) at Kapiti Landing and the effects on the roundabout at Friendship Place/Kāpiti Road have been assessed.

- (g) It is unclear that the Proposed Mitigation is able to be completed entirely within the Friendship Place road reserve-the detailed plans in support of the Application do not appear to show the full extent of the Proposed Mitigation.

My reply: The Applicant has provided sufficiently detailed concept drawings that show that the works can be accommodated within road reserve.

- (h) It is unclear that the Proposed Mitigation is adequate or appropriate, and in particular it is unclear that the length of the 10m additional approach lane is adequate or appropriate.

My reply: The Applicant has carried out an assessment by modelling the effect of the mitigation on the capacity of the roundabout and found that the mitigation is effective in improving the assessed LOS with development on the existing roundabout with no mitigation.

- (i) It is unclear that the Proposed Mitigation is adequate or appropriate if the traffic modelling needs to be updated to consider consented, but not yet constructed, development in the vicinity (including within Kapiti Landing).

My reply: The modelling includes the consented developments at Kapiti Landing and the effects on the roundabout at Friendship Place/Kāpiti Road have been assessed.

- (j) It is unclear whether the appropriateness of allowing right turn entry from Kāpiti Road to the secondary access to the Site has been considered by the Applicant, which may have some benefit in terms of reducing pressure on the Friendship Place roundabout (both to outbound Friendship Place vehicles and to eastbound Kāpiti Road vehicles).

My reply: In my opinion the provision of right turning traffic to and from the existing direct access to Kāpiti Road would have some benefit in terms of capacity at the roundabout. However, in my opinion, this would not be desirable from a road safety point of view given the volume of traffic on Kāpiti Road in combination with increased traffic to and from the proposed development and, therefore, a potential increased risk of side impact collisions.

13.3 Arising from the Application, and in particular the ITA and the transportation s92 response (Templeton Kapiti Limited (TKL) Submissions):

- (a) Traffic Counts, Traffic Volumes and Traffic Growth:

The submission questions the use of 2018 count data as applicable, and the application of negative growth based on differences between count data for a Saturday.

My reply: I have discussed these issues in Section 7.6 to 7.10 of my evidence.

- (b) Assessment Methodology: Traffic Model:

This questions the use of negative growth for the future year of 2026.

My reply: I have discussed these issues in Section 7.6 to 7.10 of my evidence.

(c) Assessment Methodology: Assessment Periods and Scenarios:

Questions around the non-use of the traffic model and lack of assessment of consented and permitted developments.

My reply: The modelling undertaken for the ITA includes the consented developments at Kapiti Landing and the effects on the roundabout at Friendship Place/Kāpiti Road have been assessed. The Kapiti Traffic Model (KTM4) does assume consented and unconsented development in the forecast years. This includes a significant amount of retail floorspace being operational at Kapiti Landing in the forecast years of 2026 and 2036 and significant road links being in place in 2026, these are the Paraparaumu Town Centre Link and the Ihakara Street extension as shown below.



Figure 3-1: Paraparaumu Town Centre Link and Development Network



Figure 3-2: Kāpiti Landing Network – 2026 [Black] and 2036 [Grey]

These elements combined in KTM4 result in an overall reduction in vehicles per day on the sections of Kapiti Road to the east and west Friendship Place/Kapiti Road roundabout in 2026 when compared to the 2018 base year. My view is that the additional assessment work carried out by the Applicant in assuming zero growth (based on traffic count data) in 2026 is appropriate and allows for a conservative assessment of the supermarket effects at the roundabout. A key piece of information to note is that the KTM4 model is a weekday model and therefore would only be applicable to use in the assessment of the PM peak period. Council does not have a weekend traffic model for Kapiti Road. As shown by traffic count data the Saturday period is the critical period for the supermarket and Kapiti Road in terms of hourly flow volumes and in the absence of a detailed weekend model, the approach taken by the Applicant for the assessment of transportation effects is appropriate in my opinion.

(d) External Vehicle Access Points:

Questions around the effects on queues and delays at the Friendship Place/Kāpiti Road roundabout and how the secondary access point to the site as a left in/left out access would be implemented.

My reply: The traffic flows for peak periods are set out in Section 7.1 of my evidence, the issues around growth and the Saturday peak is discussed in Section 7.6 to 7.12 of my evidence. The Applicant has submitted sufficiently detailed concept plans which in my opinion demonstrates that the left in/left out access can be implemented.

(e) Kāpiti Road / Friendship Place Roundabout – Mitigation:

Questions whether the mitigation proposed for the Friendship Place arm of the roundabout can be implemented within the road reserve that meets design standards.

My reply: The Applicant has provided sufficiently detailed concept drawings that show that the works can be accommodated within road reserve. The roundabout mitigation will be subject to detailed engineering design in accordance with Austroads³ standards and be subject to a detailed and post construction road safety audit process.

(f) Servicing:

Question on swept paths and the size of vehicles assessed

My reply: The Applicant has provided swept path analysis for a semi-trailer and B-Train entering and exiting the site, the swept paths include the 0.5m clearance. In my opinion the Applicant has demonstrated that the development can be serviced by the large vehicles associated with a supermarket operation.

(g) KCDP Part 2 District Wide Matters/Transport Rules:

Question around the Friendship Place/Kāpiti Road arm serving the development not complying with design standards.

My reply: The Applicant has provided swept path analysis for a semi-trailer and B-Train entering the roundabout, the swept paths include the 0.5m clearance. The roundabout alterations will be subject to detailed engineering design in accordance with Austroads⁴ standards and be subject to a detailed and post construction road safety audit process which should provide for a suitable design to be developed.

14. CONCLUSION

14.1 The proposal is for Kapiti Retail Holdings Limited to establish a Countdown supermarket and two small trade outlets on a site at 160 Kāpiti Road, Paraparaumu. I consider that the site is a suitable one for a supermarket, from a transport planning point of view. The traffic that will be generated by the proposed supermarket can be accommodated satisfactorily at the modified Friendship Place/Kāpiti Road roundabout and left in/left out Kāpiti Road access without compromising the safe and efficient operation of Kāpiti Road, noting the possible future mitigation of creating a dedicated left turn lane at western arm of the roundabout should monitoring of the supermarkets traffic effects (as required by consents conditions) determine that these mitigation measures are required. Adequate and appropriate car parking and servicing arrangements can be achieved on the site with minimal impact on surrounding activities. The on-site layout of circulation and parking, including the service area, will comply with the standards in the Kapiti Coast District Plan, and will enable the site to function with minimal impact on the surrounding area.

14.2 Upgrading of the crossing points by providing refuge islands at both the roundabout arm into the site and the left in/left out access and provision a pedestrian link across the car park will provide safe access from Kāpiti Road. Alternative travel modes (other than the private car) are also further encouraged by the provision of secure covered cycle parking for customers or staff. The Applicant has indicated that it would be feasible to provide new bus stops on both sides of Kāpiti Road to the west of the left in/left out access. If implemented this

³ Austroads Guide to Road Design Part 4B: Roundabouts

⁴ Austroads Guide to Road Design Part 4B: Roundabouts

would further encourage travel by non-car modes particularly staff employed at the site. The provision of bus stops at this location is supported by Council however, Greater Wellington Regional Council are the approving authority for public transport changes such as new bus stops and the Applicant would need to liaise with them directly to gain their approval, Council can assist with this process.

14.3 The Applicant is also proposing a Travel Plan which, if implemented, would assist and support travel to the site by non car modes. Council is fully supportive of this initiative and a suitably worded consent condition to formalise the travel plan requirement is recommended.

14.4 I conclude that the proposed supermarket development is acceptable from an overall transportation point of view, and that the effects will be less than minor.

15. RECOMMENDATION

15.1 I concur with the recommendation contained within the officer's section 42A report that the proposed development should be approved.

Neil Trotter CPEng, CMEngNZ, IntePE(NZ)

Transport Safety Leader

Kapiti Coast District Council

10 February 2022

Appendix D

Recommended Conditions

General

1. The proposed activity shall be undertaken in general accordance with:

- Woodhams Meikle Zhan Architects Plans:
 - Proposed Site Plan, Job No. 2026-73, DWG No. RMA-101, Rev. G
 - Elevations, Job No. 2026-73, DWG No. RMA-102, Rev. D
 - Cross Sections, Job No. 2026-73, DWG No. RMA-103, Rev. C
- Tim Kelly Transportation Planning Limited Plans:
 - Indicative Road Layout Changes, Drawing No. 21-005-SK001, Rev. C
 - Vehicle Tracking Plan, Drawing No. 21-005-SK002, Rev. C
 - Vehicle Tracking Plan, Drawing No. 21-005-SK003, Rev. C
 - Vehicle Tracking Plan, Drawing No. 21-005-SK004, Rev. C
 - Vehicle Tracking Plan, Drawing No. 21-005-SK005, Rev. C
- Maven Associates Plans:
 - Proposed Carpark Flood Volume Plan, Project No. 109022, Drawing No. C202, Rev. A
 - Proposed Earthworks Plan, Project No. 109022, Drawing No. C203, Rev. A
 - Proposed Flood Storage Tank Earthwork Plan, Project No. 109022, Drawing No. C204, Rev. A
 - Proposed Retaining Plan, project No. 109022, Drawing No. C220, Rev. A
 - Proposed South-East Retaining Wall Long Section, Project No. 109022, Drawing No. C221, Rev. A
 - Proposed Carpark Plan, Project No. 109022, Drawing No. C310, Rev. A
 - Proposed Carpark Plan, Project No. 109022, Drawing No. C311, Rev. A
 - Proposed Carpark Plan, Project No. 109022, Drawing No. C312, Rev. A
 - Proposed Carpark Plan, Project No. 109022, Drawing No. C313, Rev. A
 - Proposed Carpark Plan, Project No. 109022, Drawing No. C314, Rev. A
 - Proposed Carpark Cross Sections, Project No. 109022, Drawing No. C320, Rev. A
 - Site Overview Plan, Project No. 109022, Drawing No. C400, Rev. A
 - Stormwater Flooding Cross Sections, Project No. 109022, Drawing No. C401, Rev. A
 - Stormwater Flooding Cross Sections, Project No. 109022, Drawing No. C402, Rev. A
 - Truck Access Long Section, Project No. 109022, Drawing No. C403, Rev. A
 - Proposed Private Services Plan, Project No. 109022, Drawing No. C410, Rev. B

- Proposed Services Standard Details, Project No. 109022, Drawing No. C490, Rev. A
- Proposed Private Services Plan, Project No. 109022, Drawing No. C430, Rev. A
- Proposed Private Services Plan, Project No. 109022, Drawing No. C431, Rev. A
- Proposed Sediment Control Plan, Project No. 109022, Drawing No. C205, Rev. A
- Proposed Sediment Control Details, Project No. 109022, Drawing No. C206, Rev. A

All stamped as 'Final Approved Plans' on XX April 2022 and the information specifications lodged with the application RM210151 and the further information request responses provided by Forme Planning Limited on 3, 9 and 12 August, 30 September and 5 October 2021 except where modified by conditions of consent.

2. The consent holder shall comply with the requirements of the Kapiti Coast District Council's (KCDC's) Subdivision and Development Principles and Requirements 2012 (SDPR: 2012), unless alternatives are proposed by the consent holder and accepted by the Council's Development Engineer.
3. Any illumination of signs shall comply with the following:
 - a. All relevant Civil Aviation Authority Requirements; and
 - b. Light levels shall not exceed 10 lux, measured 1.5m inside the boundary of any adjoining Residential Zone.
4. Mitigation of flood hazard to create flood free building area and construction of compensatory storage must be undertaken in accordance with the details and specifications within the *Infrastructure Report* prepared by Maven Associates, dated 2 July 2021 and submitted to Council in relation to application RM210151 except where modified by conditions of consent.

Prior to the Commencement of Works

5. The consent holder shall submit copies of the plans and specifications for the engineering development for approval to the satisfaction of the Council's Development Engineer. The engineering development must be in accordance with Paragraphs 1 to 5 of Schedule 1 contained in Part 4 of the Kapiti Coast District Council's Subdivision and Development Principles and Requirements, 2012. No works shall commence until the plans are approved by KCDC's Development Engineer.

Note: Engineering drawings shall contain sufficient detail to clearly illustrate the proposal to enable assessment of compliance with the Kapiti Coast District Council's Subdivision and Development Principles and Requirements, 2012, to enable accurate construction and show individual service connections to each tenancy.
6. The consent holder shall provide the Council's Development Engineer with the names of the Developer's or Owner's Representative(s) appointed in terms of Clause B(ii) of Part 3 of the Kapiti Coast District Council's Subdivision and Development Principles and Requirements, 2012.
7. The consent holder shall advise the names and professional qualifications of any Suitably Qualified Persons required in terms of Clause B(iii) of Part 3 of the Kapiti Coast District Council's Subdivision and Development Principles and Requirements 2012.

Suitably Qualified Persons are required for, but not necessarily limited to, the following areas:

- Civil Engineering
- Stormwater Design and construction
- Water and wastewater design & construction

Note: If the Council considers any of the nominated persons are not acceptable then the consent holder shall nominate alternative persons, or the Council may require the consent holder to employ a specified Suitably Qualified Person or Persons at the consent holder's cost.

8. At least 20 working days prior to the commencement of works, the consent holder shall submit for approval in writing by Council's Development Engineer and Access and Transport Manager, a Construction Management Plan (CMP) which shall include the following:
 - a. Details of control of mud and detritus from the site onto the road – onsite wheel washing and offsite road sweeping.
 - b. Details of onsite turning for delivery vehicles.
 - c. Site compound location shown on a plan.
 - d. Identified areas for site offices and site operative parking.
 - e. Mitigation for the prevention of discharge of any material beyond the boundary of the subject site.
 - f. Noise controls and hours of construction.
 - g. Stormwater runoff.
 - h. The matters outlined in the Ministry for the Environment's Contaminated Land Management Guidelines No.1 Reporting on Contaminated Sites in New Zealand (Revised 2011).
9. All earthworks and site investigations and remediation shall be undertaken in accordance with the approved CMP and Site Remedial Action Plan required under condition 22.
10. No works shall commence until the CMP required under condition 8 has been approved in writing by Council's Development Engineer.
11. The consent holder shall comply with the requirements of the approved CMP. Any proposed amendments to the CMP shall be submitted to the Council's Development Engineer for consideration and approval. No work shall commence until amendments to the CMP have been approved by the Council's Development Engineer in writing.
12. The consent holder shall provide a detailed design site plan to Council's Development Engineer for approval that shows diversity in surface treatments/changing materials for the construction of the customer car parking area fronting Kapiti Road, to enhance the visual appearance of the site.
13. A Landscape Management Plan (LMP) shall be provided at least 20 working days prior to the purchasing of plants for approval by Council's Development Engineer. The LMP shall contain the following:
 - Existing vegetation to be retained;
 - Any vegetation to be removed;

- The extent of planting, paved (impermeable) surfaces and other landscaping elements;
 - Details of plant species that shall be native to the Foxton Ecological District;
 - Location of plants;
 - Number of plants;
 - Plant grade sizes;
 - An implementation plan describing the methods of soil preparation, details of drainage, fertilising, mulching, spraying, irrigation, staking tree pits, ongoing maintenance, replacing of dead/poorly performing plants and weed and pest management;
 - Scheduling of work, including maintenance to ensure successful establishment; and,
 - The location, height, and type of fencing.
14. The consent holder shall investigate the actual material and condition of the existing 80mm private sewer rising main and provide the findings to Council's Development Engineer as part of the detailed design. If the private sewer rising main is found to be in poor condition, the pipe shall be upgraded with the necessary details provided to Council as part of the required building consent.
15. Prior to the commencement of works, detailed design of the improvements to the Friendship Place/Kapiti Road roundabout and the secondary access to Kapiti Road shall be provided for approval in writing by Council's Access and Transport Manager. No further construction shall commence onsite until the improvement works have been completed.

Geotechnical

16. Following the demolition of the existing building and prior to construction of the new building, onsite geotechnical investigations will be undertaken and an updated geotechnical assessment, including recommendations for foundation design to mitigation any liquefaction risk prepared by a suitably qualified person shall be submitted to Council's Development Engineer. All building works on the site shall take into account the findings of the approved report.

Engineering

17. The consent holder shall notify Council's Development Engineer prior to commencement of the following stages of work, so that the Council's Development Engineer, or their authorised representative, are present on site to inspect certain stages of the works. These stages are as follows:
- Commencement of works or recommencement after a substantial lapse;
 - Water reticulation connections and services prior to back fill;
 - Wastewater services and construction of new manholes prior to back fill;
 - Completed earthworks and prepared subgrade (roading and footpaths, if any);
 - Final inspection.
18. The development shall have water supply with strainer meter and RPZ which complies with the requirements of OIML R49 (International Organization of Legal Metrology

R49:2006 Water Meters Intended for the Metering of Cold Potable Water and Hot Water - Parts 1 to 3).

Note: The Consent Holder's attention is drawn to the 'Approved Water Supply Products & Materials List, WS-10: Water Meters' (<http://www.kapiticoast.govt.nz/Planning/Resource-Consents/Standard-Drawing/Water-Standard-Drawings>). Installing an approved water meter is a means of compliance with this condition.

19. Firefighting requirements for the development shall comply with the New Zealand Fire Service Firefighting Water Supplies Code of Practice SNZ PAS 4509:2008
20. Any unused existing water service connections being abandoned shall be capped at the main.
21. Any unused existing wastewater service connections being abandoned shall be capped at the main.

Contaminated Soil

22. A Detailed Site Investigation (DSI) report detailing the findings of onsite investigation works (soil sampling) following the demolition of the existing building, and a site remedial action plan, shall be submitted at least 20 working days prior to the commencement of the construction works consented under RM210151 for approval in writing by Council's Development Engineer.

Note: The DSI report, remedial action plan and monitoring and management plan shall cover the matters outlined in the Ministry for the Environment's Contaminated Land Management Guidelines No.1 Reporting on Contaminated Sites in New Zealand (Revised 2011).

23. Within 20 working days of the completion of remedial works on the site, a Site Validation Report (SVR) shall be provided to Council. The SVR shall be prepared by a suitably qualified contaminated land professional in accordance with the Ministry of Environment Contaminated Land Management Guidelines, No 1 Reporting and Contaminated Sites in New Zealand and No. 5 Site Investigation and Analysis of Soils. The person preparing the report shall also provide a statement certifying that all works have been carried out in accordance with the requirements of the consent.

Transport

24. Detailed Design and Post Construction road safety audits are to be provided upon completion of works and are required for the proposed alterations to Friendship Place/Kapiti Road roundabout and the secondary access to Kapiti Road.

Note: The road safety audits are to be carried out in accordance with guidance contained in the Kapiti Coast District Council Subdivision and Development Principles and Requirements 2012 and Waka Kotahi (NZTA) guidance.

25. The Post Construction road safety audit shall be provided to Council's Access and Transport Manager for approval in writing at least 20 working days prior to the opening of the supermarket.
26. Any required signage/road markings must be provided in accordance with TCD's, The Manual for Traffic Signs and Signals: 2010 and Traffic Control Devices Manual: 2008.
27. A workplace travel plan shall be submitted to Council's Access and Transport Manager for approval in writing at least 20 working days prior to the opening of the supermarket,

the travel plan shall outline measures, facilities, and incentives to encourage non-car travel to and from the site.

28. Delays and Level of Service (LOS) at the Kapiti Road (western) arm of the Friendship Place/Kapiti Road roundabout (the roundabout) shall be monitored for a period of 24 months after opening of the supermarket, the process for monitoring, to be carried out by Applicant, is as follows:
- a. A classified turning traffic count for all movements at the roundabout shall be undertaken on a Saturday during an agreed time period and month prior to the commencement of construction to form the baseline traffic counts and an assessment undertaken using SIDRA by a suitably qualified transportation engineer.
 - b. 12 months after commencement of operation of the Supermarket - A classified turning traffic count for all movements at the roundabout shall be undertaken on a Saturday during an agreed time period and month prior to the commencement of construction to form the baseline traffic counts and an assessment undertaken using SIDRA by a suitably qualified transportation engineer.
 - c. 24 months after commencement of operation of the Supermarket - A classified turning traffic count for all movements at the roundabout shall be undertaken on a Saturday during an agreed time period and month prior to the commencement of construction to form the baseline traffic counts and an assessment undertaken using SIDRA by a suitably qualified transportation engineer.

If the results of the capacity tests for b) or c) above show the LOS falling below C on the western Kapiti Road arm of the roundabout, then mitigation works in the form of widening the western Kapiti Road arm of the roundabout shall be progressed by the Applicant through detailed design and implementation (at the Applicants cost) in accordance with details to be submitted and approved in writing to the Access and Transport Manager.

Advice Notes:

- The consent holder shall notify the Council's RMA Compliance Officer of the start and completion dates of the works in writing 48 hours before the works are carried out. The consent holder shall fill out and return (by email to the duty compliance officer at compliance.dutyofficer@kapiticoast.govt.nz, or by fax to (04) 2964 830 or by post to Private Bag 60601, Paraparaumu) the form that is attached to the decision letter.
- The consent holder shall pay to the Kapiti Coast District Council the actual and reasonable costs associated with the monitoring of conditions (or review of consent conditions), or supervision of the resource consent as set in accordance with Section 36 of the Resource Management Act 1991. These costs* may include site visits, correspondence and the actual costs of materials or services which may have to be obtained.

*Please refer to Kapiti Coast District Council's current schedule of Resource Management fees for guidance on the current hourly rate chargeable for Council's staff.

- Under Section 125 of the Resource Management Act 1991, this resource consent will lapse in five years, unless it is given effect to within that time.
- It is the consent holder's responsibility to comply with any conditions imposed on this resource consent prior to and during (as applicable) exercising this resource consent.

- Please note that a resource consent is not a consent to build. A building consent must be issued prior to any building work being undertaken.
- If you disagree with any of the above conditions or disagree with the additional charges relating to the processing of the application, you have a right of objection pursuant to sections 357A or 357B of the Resource Management Act 1991. Any objection must be made in writing to the council within 15 working days of notification of the decision.
- The consent holder is responsible for obtaining all other necessary consents, permits, and licences, including those under the Building Act 2004, and the Heritage New Zealand Pouhere Taonga Act 2014. This consent does not remove the need to comply with all other applicable Acts (including the Property Law Act 2007 and the Health and Safety in Employment Act 1992), regulations, relevant Bylaws, and rules of law. This consent does not constitute building consent approval. Please check whether a building consent is required under the Building Act 2004.
- Development Contributions will be required pursuant to Section 198 of the Local Government Act 2002 and the Council's Development Contributions Policy 2021 when creating additional gross floor area for commercial premises. The contributions will be calculated and levied for each square meter of gross floor area over the existing 3,700m² floor area of building on the site.

The Development Contributions Policy 2021 defines *gross floor area* as:

The sum of the gross area of all floors of all buildings on a property, measured from the exterior faces of the exterior walls or from the centre lines of walls separating two buildings. Gross floor area also includes covered yards and areas covered by a roof but not enclosed by walls. The gross floor area of a building shall not include:

- *Uncovered stairways;*
- *Floor space in terraces (open or roofed), external balconies, breezeways or porches;*
- *Roof car parking, lift towers and machinery rooms on the roof having a floor area of not more than 200m²;*
- *Car parking areas; and*
- *Floor space of interior balconies and mezzanines not used by the public.*

The total gross floor area of the development is 4,600m², accounting for the existing building on the site, an additional 900m² of gross floor area is created by this resource consent. The Development Contributions have been calculated in accordance with the fees that applied at the time the consent was lodged based on the calculation for commercial buildings at 0.002 x GFA where every 500m² is considered a residential unit equivalent. The fees are listed below:

Items	Fees including GST(NZD)
Roading & Transport - Districtwide	\$2,075.00
Water Treatment - Paraparaumu	\$6,266.00
Water Reticulation - Paraparaumu	\$1,616.00
Wastewater Treatment - Paraparaumu	\$527.00
Wastewater Reticulation - Paraparaumu	\$250.00
Stormwater - Districtwide	\$185.00

Stormwater Collection & Management - Paraparaumu	\$470.00
Community Infrastructure - Districtwide	\$1,789.00
Subtotal	\$13,178.00
Total based on the additional GFA of 900m²	\$23,720.40

The contributions must be paid prior to the issue of a Code Compliance Certificate under Section 95 of the Building Act 2004 (please refer also to Section 208 of the Local Government Act 2002).

- Works within the legal road will only be approved where they comply with Council procedures and processes which are set out below:

Before undertaking work in the legal road you must make a Corridor Access Request (CAR) and receive a Works Access Permit (WAP) from us. Some examples of activities requiring a permit are:

 - trenching works;
 - footpaths and entranceways;
 - work within the berm or shoulder of the road; and
 - tree work scaffolding and crane work.
- Before any excavations are undertaken a "Before U Dig" inquiry must be made to check for locations of any underground services. This is a web based service that you or your contractor use to get plans and information emailed out to you. This also provides the mechanism for you to make a Corridor Access Request and provide us with a Traffic Management Plan to protect your site, contractors, and the public during operations. Corridor Access Requests require 5 working days' notice before work can commence and Traffic Management Plans for road closures and events must be received 42 working days in advance of the closure or event. Please note: The "Before U Dig" service has no information on council's buried water, wastewater or stormwater assets. Our mapping tools show the location of the buried council assets.
- Work must be undertaken in accordance with Councils guides and standard drawings. Examples of forms, guides and standards drawings an information sheet, application form and standard drawings (engineering plans) are available for download or print from the Council website and examples include:
 - Vehicle Installation Information;
 - Vehicle Crossing Application Form;
 - Roading Standard Drawings; and
 - Vehicle Crossing Guidelines.
- Evidence of archaeological sites may include kōiwi (human skeletal remains), taonga Māori (Māori artefacts), oven stones, charcoal, shell middens, ditches, banks, pits and old building foundations. If any archaeological site(s) are uncovered during physical works, Ātiawa ki Whakarongotai Charitable Trust will require the contractor to adopt the following protocols:
 - a. Work shall cease immediately within 100 metres of the site of discovery.

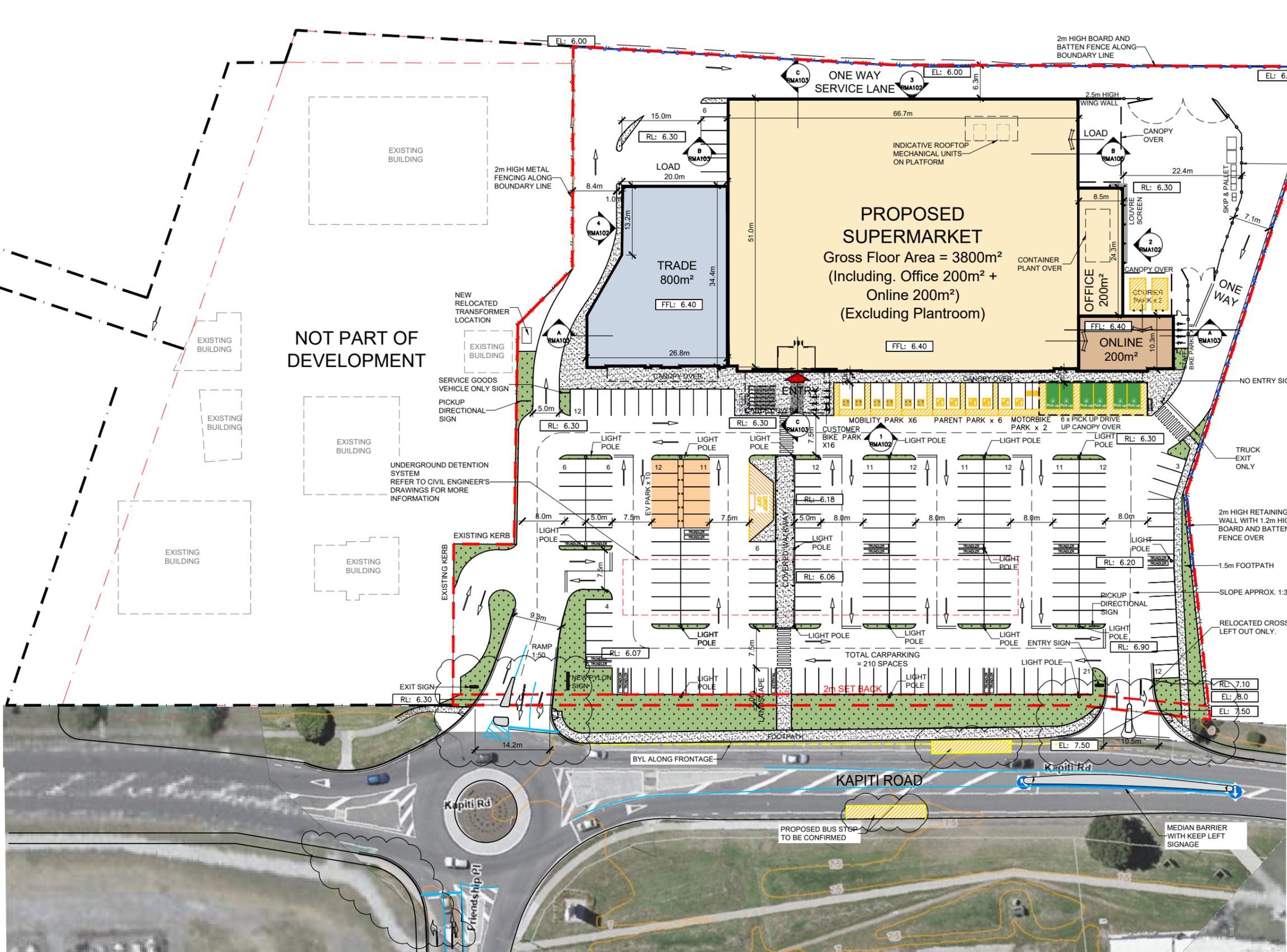
- b. The contractor and subcontractor(s) must shut down all machinery, isolate and secure the site, and advise the project manager.
- c. No materials relating to the artefacts or site shall be removed.
- d. The project manager shall promptly advise Ātiawa ki Whakarongotai Charitable Trust.
- e. If skeletal remains are uncovered, the project manager will also advise New Zealand Police.
- f. An archaeologist approved by Ātiawa ki Whakarongotai Charitable Trust shall be employed at the expense of the contractor to examine and record the site.
- g. Ātiawa ki Whakarongotai Charitable Trust will at their discretion contact other iwi groups and organise a site inspection by appropriate tangata whenua advisors and the archaeologist.
- h. If as a result of the site inspection and investigation there is a need for an appropriate ceremony, Ātiawa ki Whakarongotai Charitable Trust will arrange such at the contractor's expense.
- i. Materials discovered will be handled and removed by the Ātiawa ki Whakarongotai Charitable Trust representatives responsible for the tikanga appropriate to their removal and preservation, or re-interment.
- j. Works affecting the archaeological site shall not resume until Ātiawa ki Whakarongotai Charitable Trust, and the New Zealand Police in the case of skeletal remains, have given the appropriate consent, approval or authority for work to continue. The contractor and subcontractor(s) will allow representatives of Ātiawa ki Whakarongotai Charitable Trust and the archaeologist all reasonable access to the site to carry out their respective responsibilities or activities under this protocol.

Contact details for iwi representatives are as follows:

Ātiawa ki Whakarongotai Charitable Trust

PO Box 509

Waikanae 5250



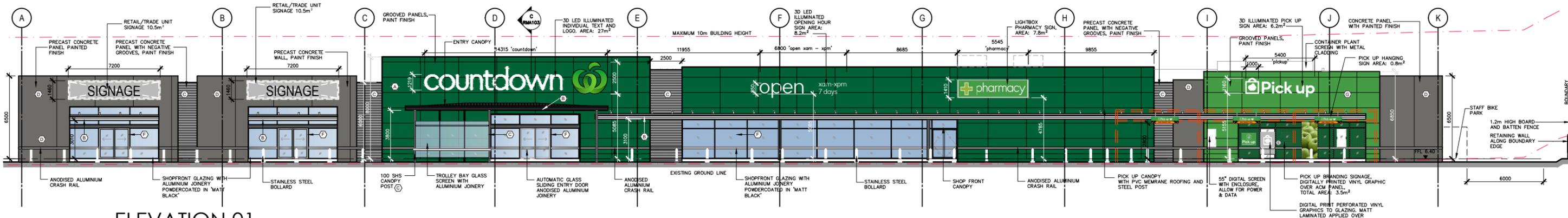
LEGAL LOT DESCRIPTION:
 LOT 1 & 2 DP 63027
 LOT 3 DP 63992

AREA SUMMARY:
 SITE AREA : 16617m²
 BUILDING AREA: 4600m² (28%)

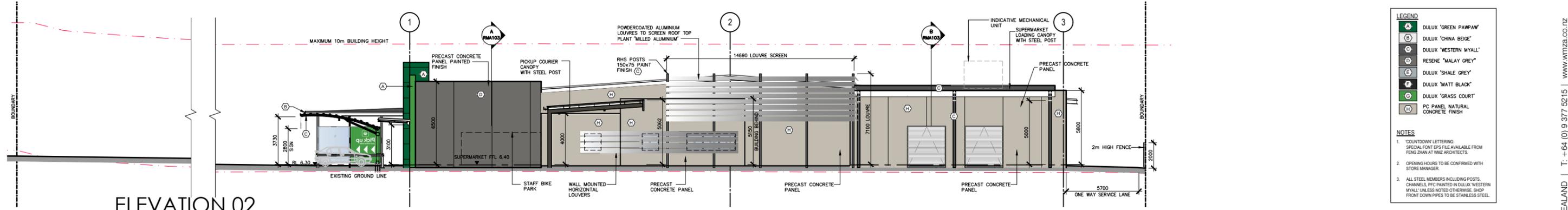
- DEVELOPMENT SUMMARY:**
- ① PROPOSED SUPERMARKET = 3800m² (Including Offices & Online, Excluding Plantrooms) (1:20 Parking Ratio = 190 Parking Lots)
 - ② PROPOSED TRADE STORE = 800m² Approx (1:40 Parking Ratio = 20 Parking Lots)
- PARKING SUMMARY:**
- ③ PARKING REQUIRED = 210 SPACES
 - ④ PARKING PROVIDED = 210 SPACES

PROPOSED SITE PLAN
 Scale 1:750 at A3

RMA ISSUE



ELEVATION 01
Scale 1:300 at A3



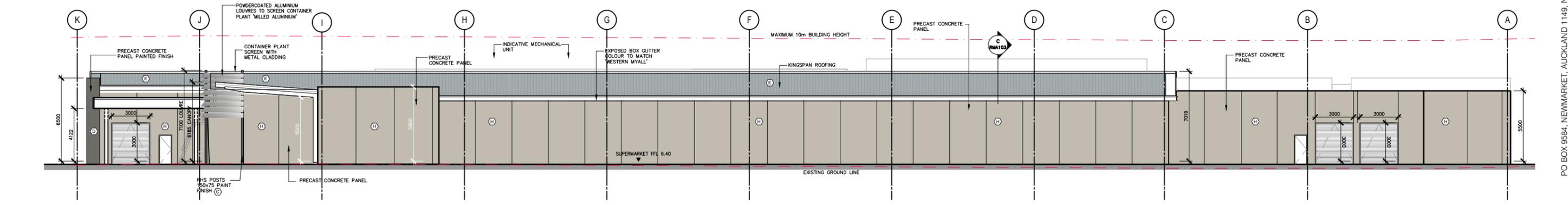
ELEVATION 02
Scale 1:300 at A3

LEGEND

- (A) DULUX 'GREEN PAWPAW'
- (B) DULUX 'CHINA BEIGE'
- (C) DULUX 'WESTERN MYALL'
- (D) RESENE 'MALAY GREY'
- (E) DULUX 'SHALE GREY'
- (F) DULUX 'MATT BLACK'
- (G) DULUX 'GRASS COURT'
- (H) PC PANEL NATURAL CONCRETE FINISH

NOTES

- 'COUNTDOWN' LETTERING: SPECIAL FONT EPS FILE AVAILABLE FROM FENG ZHAN AT W&Z ARCHITECTS.
- OPENING HOURS TO BE CONFIRMED WITH STORE MANAGER.
- ALL STEEL MEMBERS INCLUDING POSTS, CHANNELS, PFC PAINTED IN DULUX 'WESTERN MYALL' UNLESS NOTED OTHERWISE. SHOP FRONT DOWNPIPES TO BE STAINLESS STEEL.



ELEVATION 03
Scale 1:300 at A3



ELEVATION 04
Scale 1:300 at A3



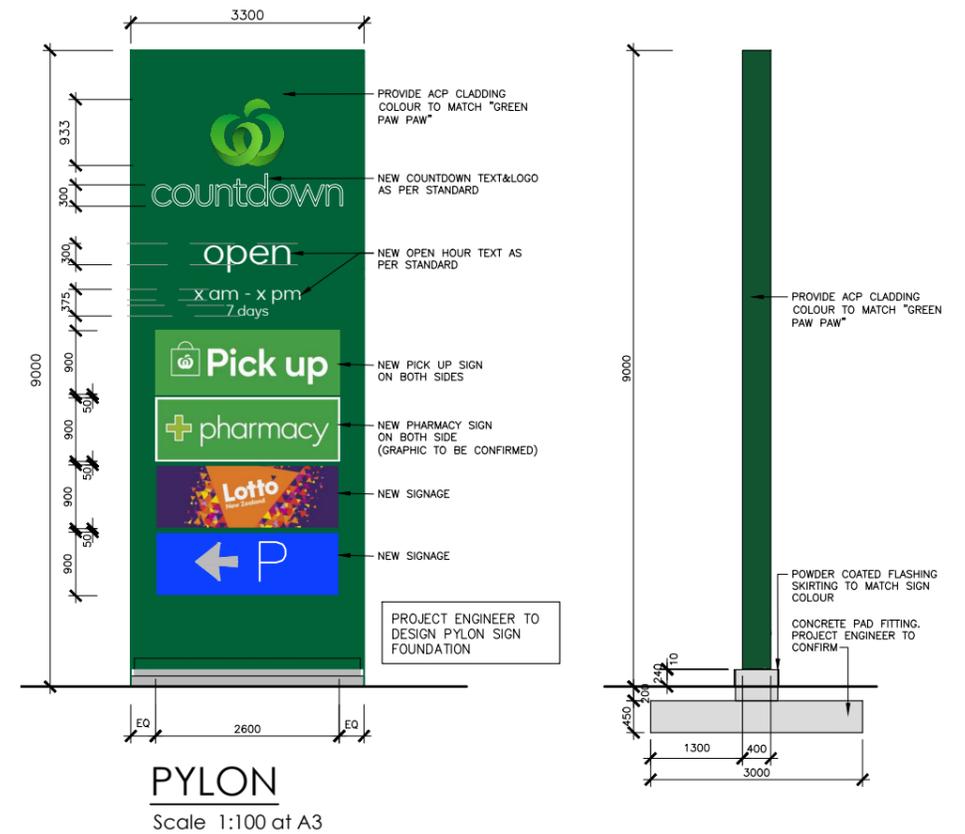
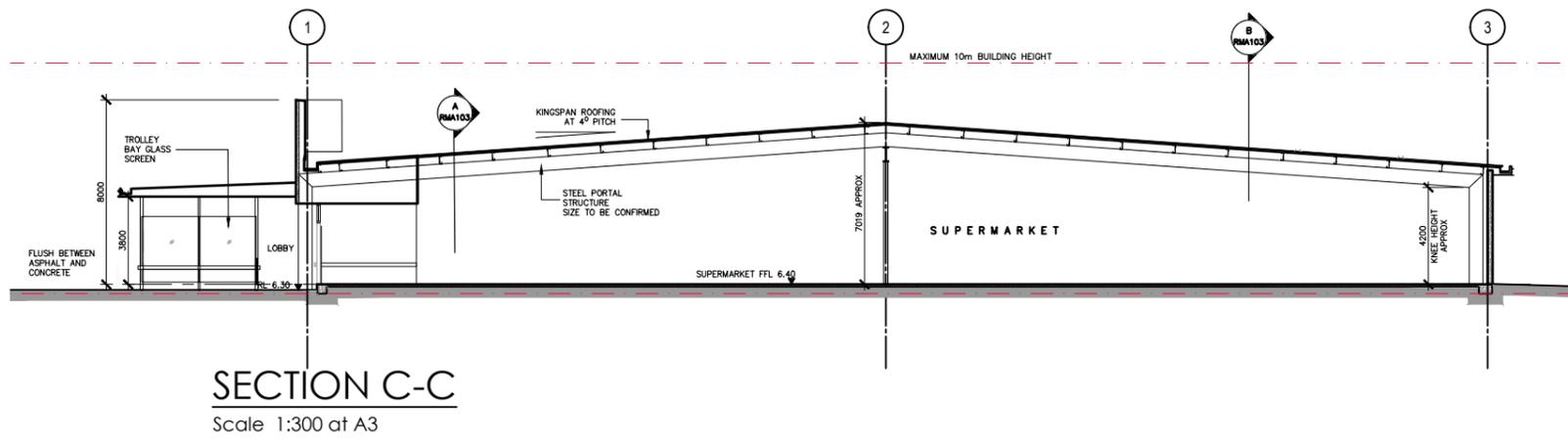
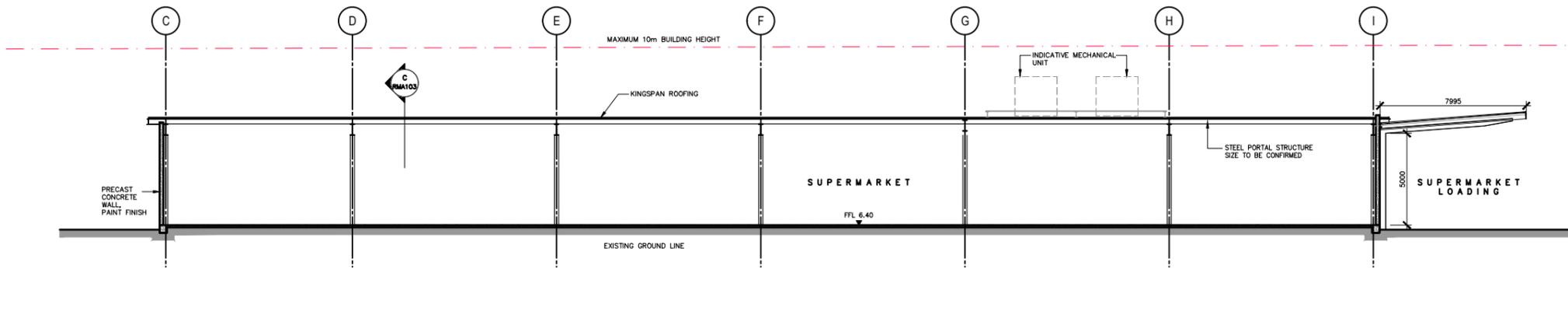
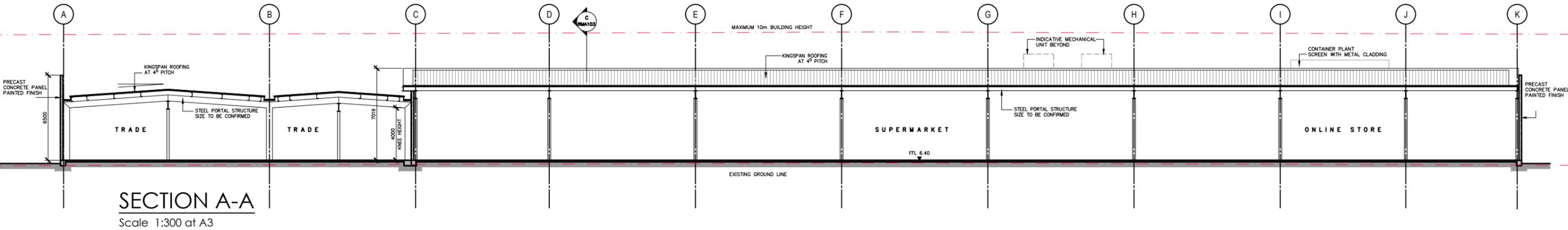
RMA ISSUE



PROPOSED SUPERMARKET RMA
Countdown 160 Kapiti Road, Paraparaumu

SCALE: 1:300 at A3
DATE: 07 JULY 2021
JOB NO: 2026-73
DWG NO: RMA-102 Rev D

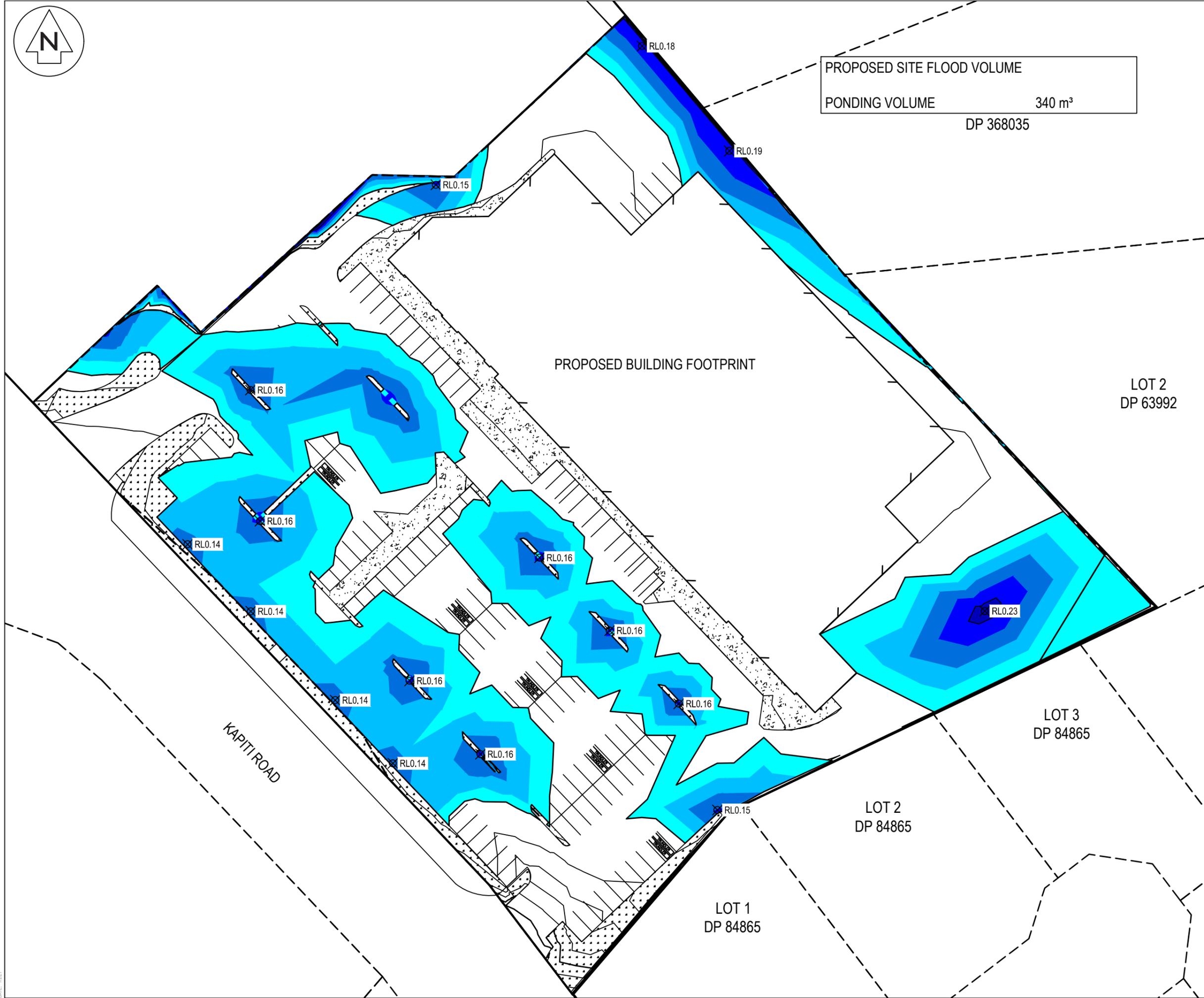




RMA ISSUE



PROPOSED SITE FLOOD VOLUME
 PONDING VOLUME 340 m³
 DP 368035



- Notes
1. All works to be in accordance with KCDP council standards.
 2. It is the contractors responsibility to locate all services that may be affected by his operations.
 3. The contractor shall comply with all relevant Health and Safety requirements.
 4. The contractor shall obtain all necessary approval from utility operators before commencing work under or near their services.
 5. Sediment control shall be installed and operational before earthworks start onsite in accordance with council standards.
 6. Contractor shall provide asbuilt of working sediment control devices and confirmation of pond/decent volumes to engineer.

Legend

EX BDY
 PROP BDY

PONDING DEPTH			
Number #	Minimum Elevation	Maximum Elevation	Color
1	0.000	0.050	Cyan
2	0.050	0.100	Light Blue
3	0.100	0.150	Medium Blue
4	0.150	0.200	Dark Blue
5	0.200	0.250	Very Dark Blue

Rev	Description	By	Date
A	RC	CA	06/2021
Survey	CUTTRISS		09/16
Design	CA		06/21
Drawn	CA		06/21
Checked	GB		06/21

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Project
**160 KAPITI ROAD
 PARAPARAUMU
 FOR
 KAPITI RETAIL HOLDINGS
 LIMITED**

Title
**PROPOSED
 CARPARK FLOOD
 VOLUME PLAN**

Project no.	109022
Scale	NTS
Cad file	109022 C200 PONDING.DWG
Drawing no.	C202
Rev	A



BULK SUBGRADE WORKS

CUT VOLUME 1,883 m³
 FILL VOLUME 1036 m³
 NET CUT 847 m³

ADDITIONAL CUT AND CART OFFSITE
 VOLUME FOR SW TANKS 1,974 m³

EARTHWORKS AREA = 16,619m² / 1.66 Ha

NOTE: NO ALLOWANCE FOR SERVICES TRENCHES

- Notes
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 6. Contractor shall provide asbuilt of working sediment control devices and confirmation of pond/decent volumes to engineer.



Legend

EX BDY
 PROP BDY

Number #	Minimum Elevation	Maximum Elevation	Color
1	-2.000	-1.500	Red
2	-1.500	-1.000	Brown
3	-1.000	-0.500	Olive Green
4	-0.500	0.000	Light Green
5	0.000	0.500	Green
6	0.500	1.000	Dark Green

Rev	Description	By	Date
A	RC	CA	06/2021
Survey	CUTTRISS		09/16
Design	CA		06/21
Drawn	CA		06/21
Checked	GB		06/21

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Project
**160 KAPITI ROAD
 PARAPARAUMU
 FOR
 KAPITI RETAIL HOLDINGS
 LIMITED**

Title
**PROPOSED
 EARTHWORKS
 PLAN**

Project no.	109022
Scale	NTS
Cad file	109022 C200 PONDING.DWG
Drawing no.	C203
Rev	A



FLOOD STORAGE CHAMBER VOLUME
 INITIAL CUT VOLUME ESTIMATE 2281 m³

LOT 63
 DP 368035

LOT 2
 DP 63992

LOT 3
 DP 84865

LOT 2
 DP 84865

LOT 1
 DP 84865

KAPITI ROAD

EXCAVATION AT 1:1 GRADE

CUT -1.67

CUT -1.68

CUT -1.68

CUT -1.68

- Notes
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 6. Contractor shall provide asbuilt of working sediment control devices and confirmation of pond/decent volumes to engineer.

Legend
 EX BDY
 PROP BDY

Cut/Fill Table			
Number #	Minimum Elevation	Maximum Elevation	Color
1	-2.000	-1.500	
2	-1.500	-1.000	
3	-1.000	-0.500	
4	-0.500	0.000	

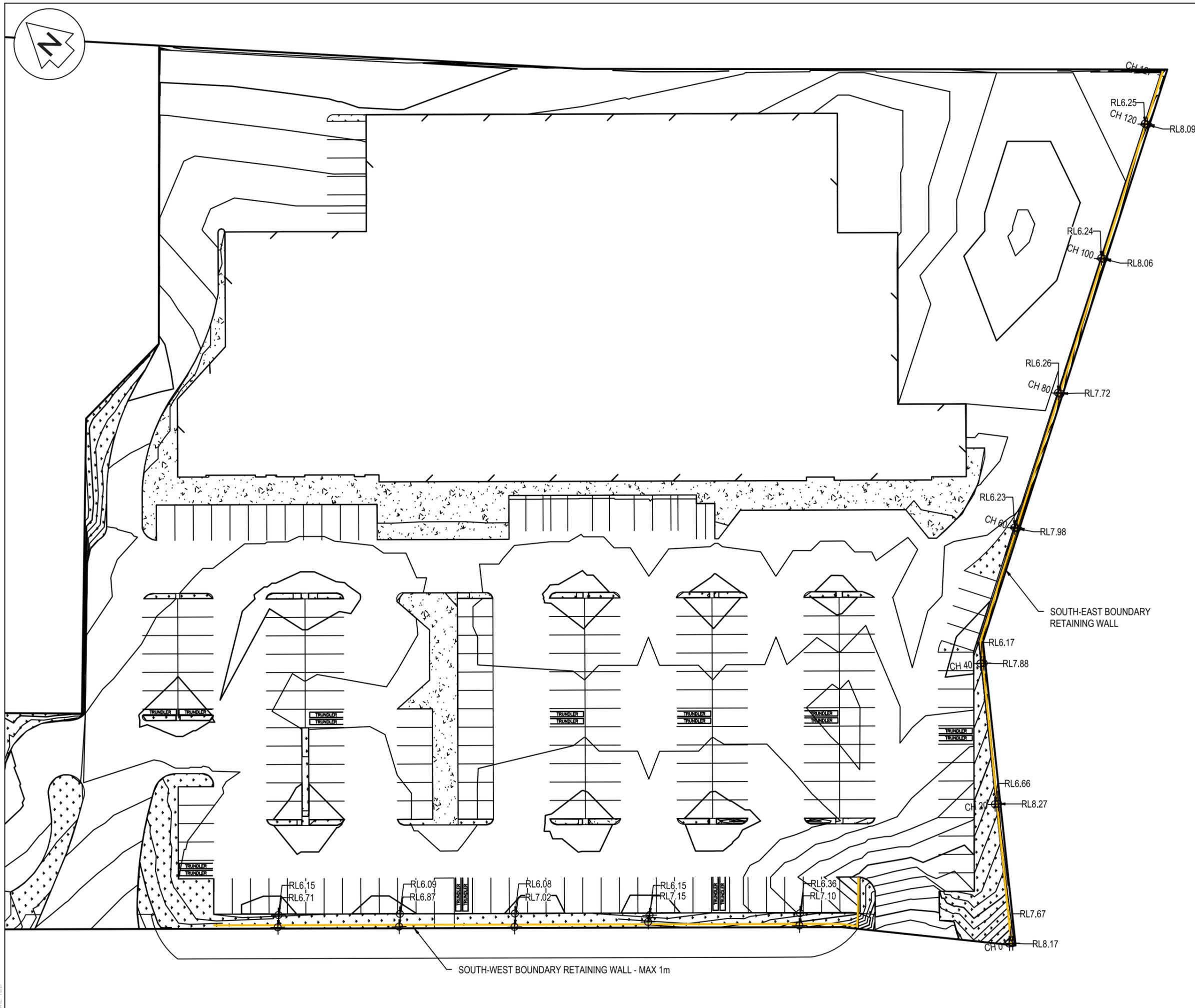
Rev	Description	By	Date
A	RC	CA	06/2021
Survey	CUTTRISS		09/16
Design	CA		06/21
Drawn	CA		06/21
Checked	GB		06/21

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Project
**160 KAPITI ROAD
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 FOR
 KAPITI RETAIL HOLDINGS
 LIMITED**

Title
**PROPOSED
 FLOOD STORAGE TANK
 EARTHWORKS PLAN**

Project no.	109022
Scale	NTS
Cad file	109022 C200 PONDING.DWG
Drawing no.	C204
Rev	A



- Notes
1. All works to be in accordance with KCDC standards.
 2. It is the contractors responsibility to locate all services that may be affected by his operations.
 3. The contractor shall comply with all relevant Health and Safety requirements.
 4. The contractor shall obtain all necessary approval from utility operators before commencing work under or near their services.

Legend

	EX BDY
	PROP BDY
	PR MAJOR CONTOUR
	PR MINOR CONTOUR
	PROP RETAINING

Rev	Description	By	Date
A	RC	CA	06/2021
Survey	CUTTRISS		09/16
Design	CA		2021
Drawn	CA		2021
Checked	BB		2021

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Project
**160 KAPITI ROAD
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 KAPITI RETAIL HOLDINGS
 LIMITED**

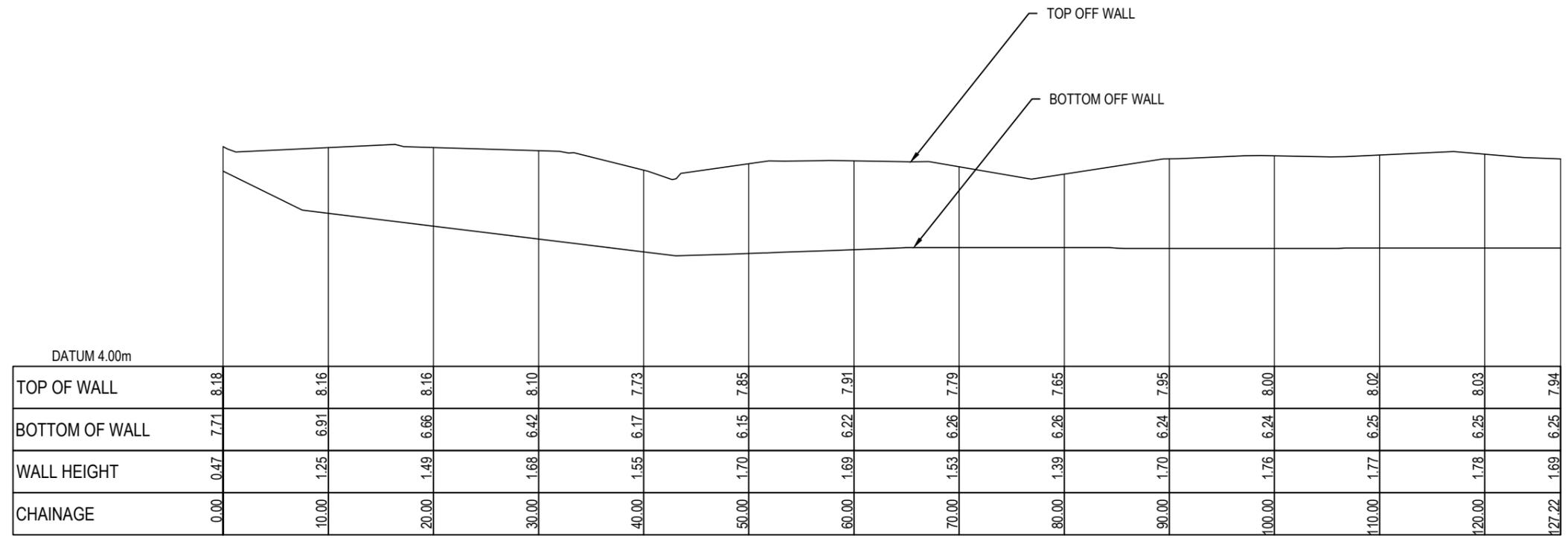
Title
**PROPOSED
 RETAINING
 PLAN**

Project no.	109022
Scale	1:500 @ A3
Cad file	109022 C220 RETAINING.DWG
Drawing no.	C220
Rev	A

- Notes
1. All works to be in accordance with KCDC standards.
 2. It is the contractors responsibility to locate all services that may be affected by his operations.
 3. The contractor shall comply with all relevant Health and Safety requirements.
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Legend

	EX BDY
	PROP BDY
	PR MAJOR CONTOUR
	PR MINOR CONTOUR
	PROP RETAINING



SOUTH EAST BOUNDARY RETAINING WALL
SCALE: HORI 1:1000 VERT 1:200

Rev	Description	By	Date
A	RC	CA	06/2021
Survey	CUTTRISS		09/16
Design	CA		2021
Drawn	CA		2021
Checked	BB		2021

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Project
**160 KAPITI ROAD
PARAPARAUMU
FOR
KAPITI RETAIL HOLDINGS
LIMITED**

Title
**PROPOSED SOUTH-EAST
RETAINING WALL
LONG SECTION**

Project no.	109022
Scale	1:500 @ A3
Cad file	109022 C220 RETAINING.DWG
Drawing no.	C221
Rev	A



LOT #2

LOT #1

PROP BUILDING FOOTPRINT

REFER C312

REFER C313

REFER C311

KAPITI ROAD

REFER C314

ENTRY / EXIT

HEAVY DUTY VEHICLE CROSSINGS REFER TO RD-006

ENTRY / EXIT

HEAVY DUTY VEHICLE CROSSINGS REFER TO RD-006

- Notes
- All works to be in accordance with KCDC standards.
 - Pipe bedding: 0 - 10% granular bedding, 10 - 20% weak concrete bedding greater than 20% weak concrete bedding (7mpa plus anti scour blocks at 6m crs).
 - Each connection shall be marked by a 50mmx50mm treated pine stake extending 600mm above ground level with the top painted. This marker post shall be placed alongside a timber marker installed at the time of pipelaying and extending from the connection to 150mm below finished ground level. Connections shall be accurately indicated on "as built" plans.
 - Approved hardfill is to be used in backfilling of all road crossings and vehicle crossings to council standards.
 - Heavy duty manhole lids and frames to be used in trafficked areas.
 - All Manholes are to be 1050mmØ unless shown otherwise.
 - All cesspit leads shall have min cover 0.9m.
 - All lines to be abandoned shall be sealed at each end. timing of all sealing to be coordinated with council staff.

Legend

	EX BDY
	PROP BDY
	ABUTTING BDY'S
	PROP SWCP SINGLE
	PROP SWCP DOUBLE
	PROP RETAINING
	PROP LANDSCAPING
	PROP CONCRETE

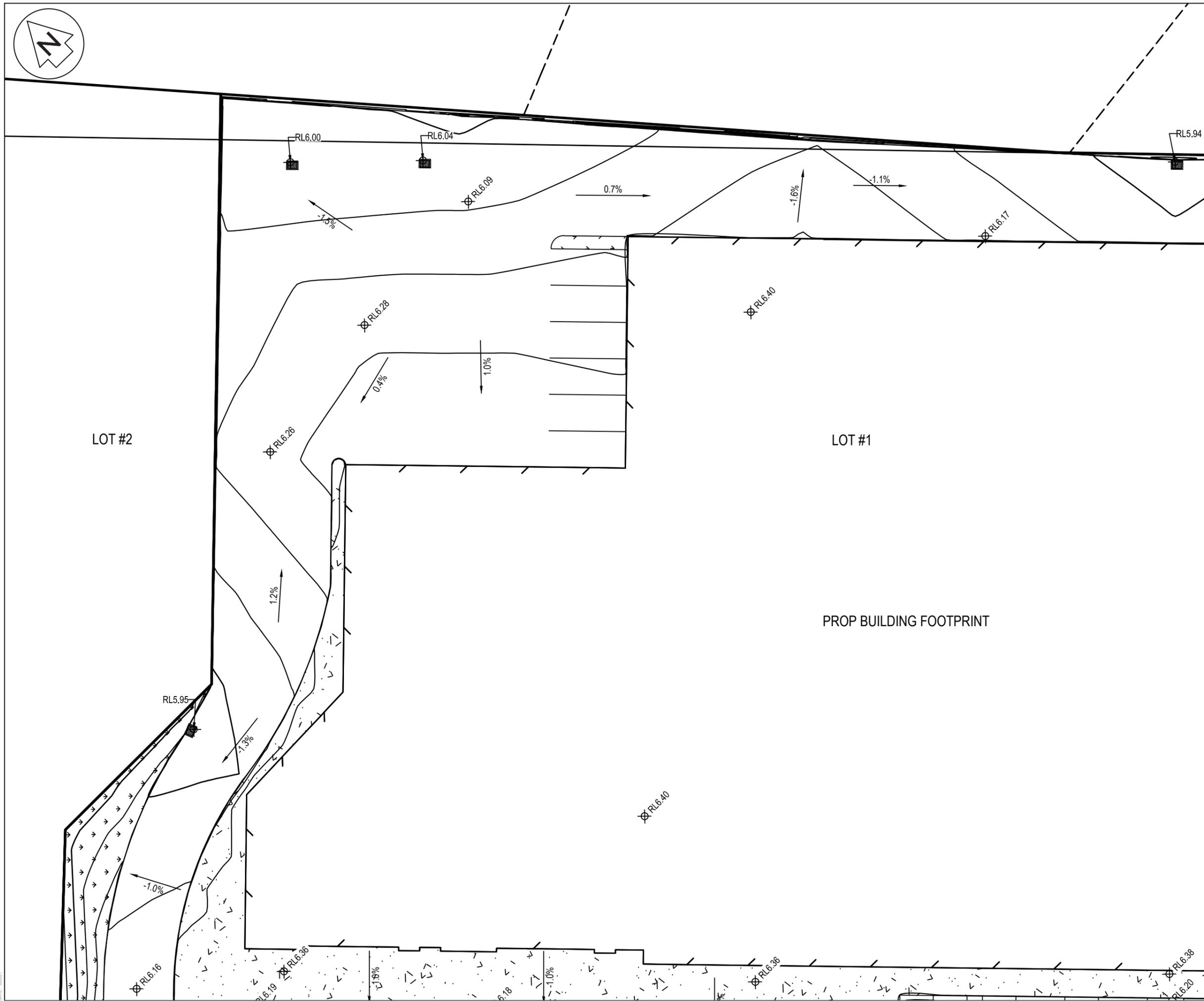
A	RC	CA	06/2021
Rev	Description	By	Date

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Project
160
KAPITI ROAD
FOR
KAPITI RETAIL
HOLDINGS LTD

Title
PROPOSED
CARPARK
PLAN

Project no.	109022
Scale	1:500 @ A3
Cad file	109022 C300 SITE PLAN.DWG
Drawing no.	C310
Rev	A



- Notes
- All works to be in accordance with KCDC standards.
 - Pipe bedding: 0 - 10% granular bedding, 10 - 20% weak concrete bedding greater than 20% weak concrete bedding (7mpa plus anti scour blocks at 6m crs).
 - Each connection shall be marked by a 50mmx50mm treated pine stake extending 600mm above ground level with the top painted. This marker post shall be placed alongside a timber marker installed at the time of pipelaying and extending from the connection to 150mm below finished ground level. Connections shall be accurately indicated on "as built" plans.
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Legend

	EX BDY
	PROP BDY
	ABUTTING BDY'S
	PROP SWCP SINGLE
	PROP SWCP DOUBLE
	PROP RETAINING
	PROP LANDSCAPING
	PROP CONCRETE

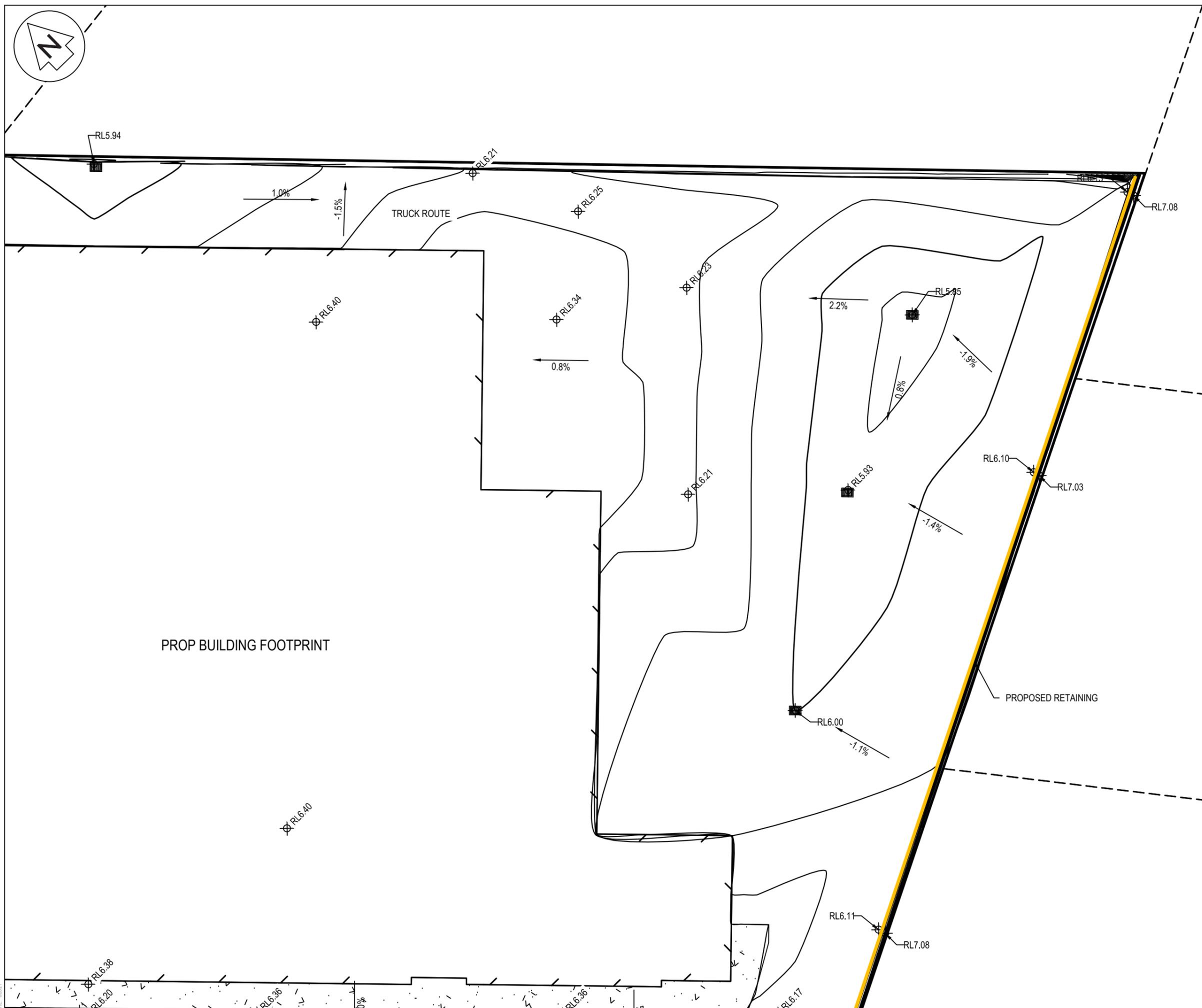
Rev	Description	By	Date
A	RC	CA	06/2021
Survey			
Design	CA		05/2021
Drawn	CA		05/2021
Checked	BB		05/2021

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Project
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KAPITI ROAD
FOR
KAPITI RETAIL
HOLDINGS LTD

Title
PROPOSED
CARPARK
PLAN

Project no.	109022
Scale	1:250 @ A3
Cad file	109022 C300 SITE PLAN.DWG
Drawing no.	C312
Rev	A



- Notes
- All works to be in accordance with KCDC standards.
 - Pipe bedding: 0 - 10% granular bedding, 10 - 20% weak concrete bedding greater than 20% weak concrete bedding (7mpa plus anti scour blocks at 6m crs).
 - Each connection shall be marked by a 50mmx50mm treated pine stake extending 600mm above ground level with the top painted. This marker post shall be placed alongside a timber marker installed at the time of pipelaying and extending from the connection to 150mm below finished ground level. Connections shall be accurately indicated on "as built" plans.
 - Approved hardfill is to be used in backfilling of all road crossings and vehicle crossings to council standards.
 - Heavy duty manhole lids and frames to be used in trafficked areas.
 - All Manholes are to be 1050mmØ unless shown otherwise.
 - All cesspit leads shall have min cover 0.9m.
 - All lines to be abandoned shall be sealed at each end. timing of all sealing to be coordinated with council staff.

Legend

	EX BDY
	PROP BDY
	ABUTTING BDY'S
	PROP SWCP SINGLE
	PROP SWCP DOUBLE
	PROP RETAINING
	PROP LANDSCAPING
	PROP CONCRETE

Rev	Description	By	Date
A	RC	CA	06/2021

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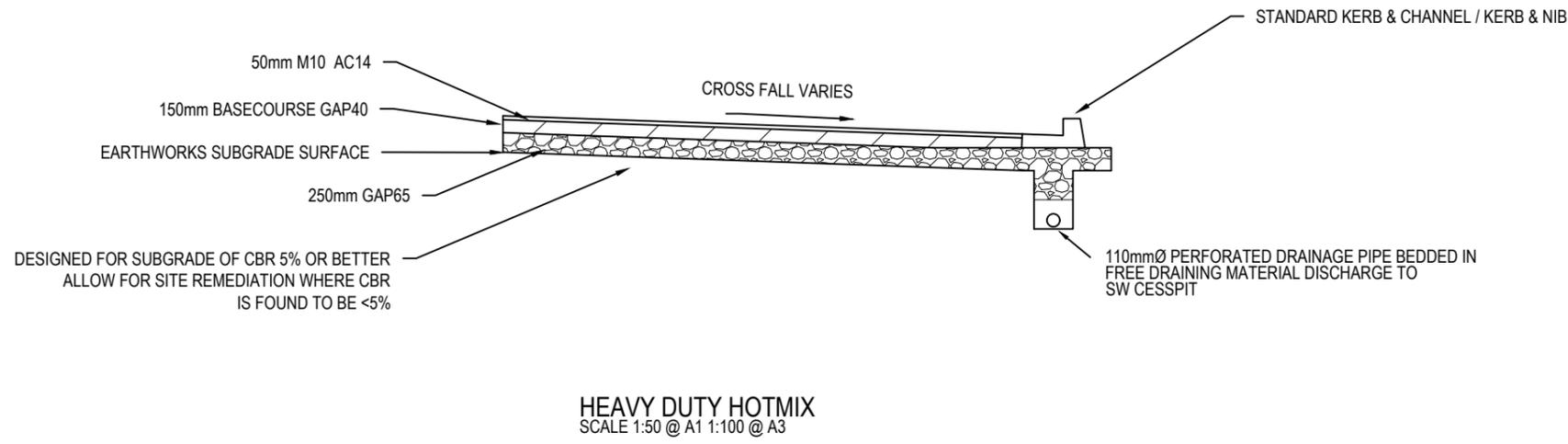
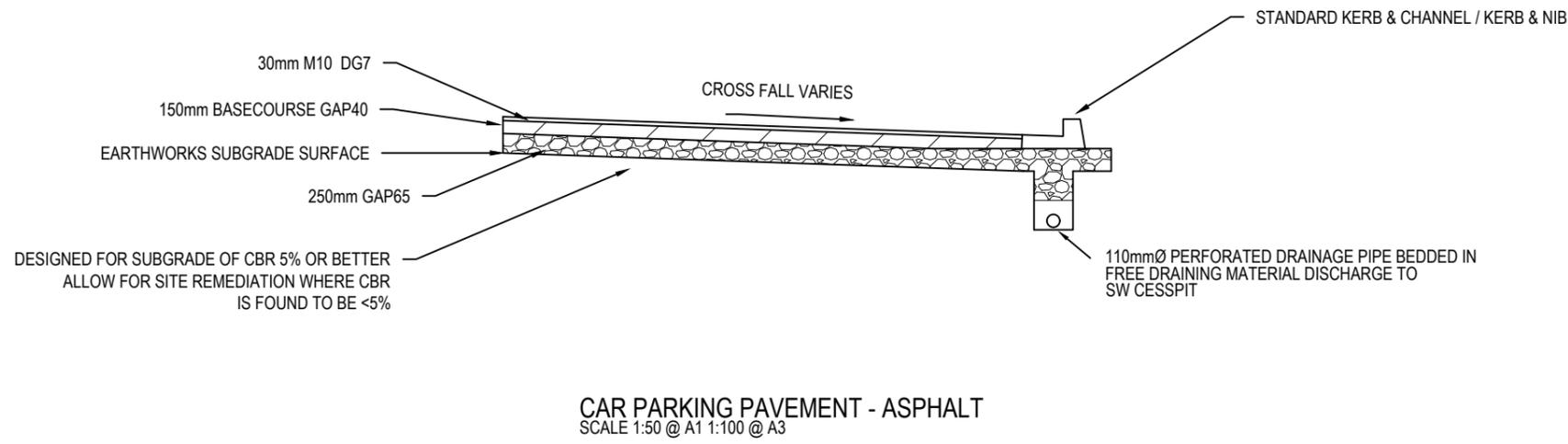
Project
160
KAPITI ROAD
FOR
KAPITI RETAIL
HOLDINGS LTD

Title
PROPOSED
CARPARK
PLAN

Project no.	109022
Scale	1:250 @ A3
Cad file	109022 C300 SITE PLAN.DWG
Drawing no.	C313
Rev	A

DATE: 02/21

Notes
 1. All works to be in accordance with KCDC standards.



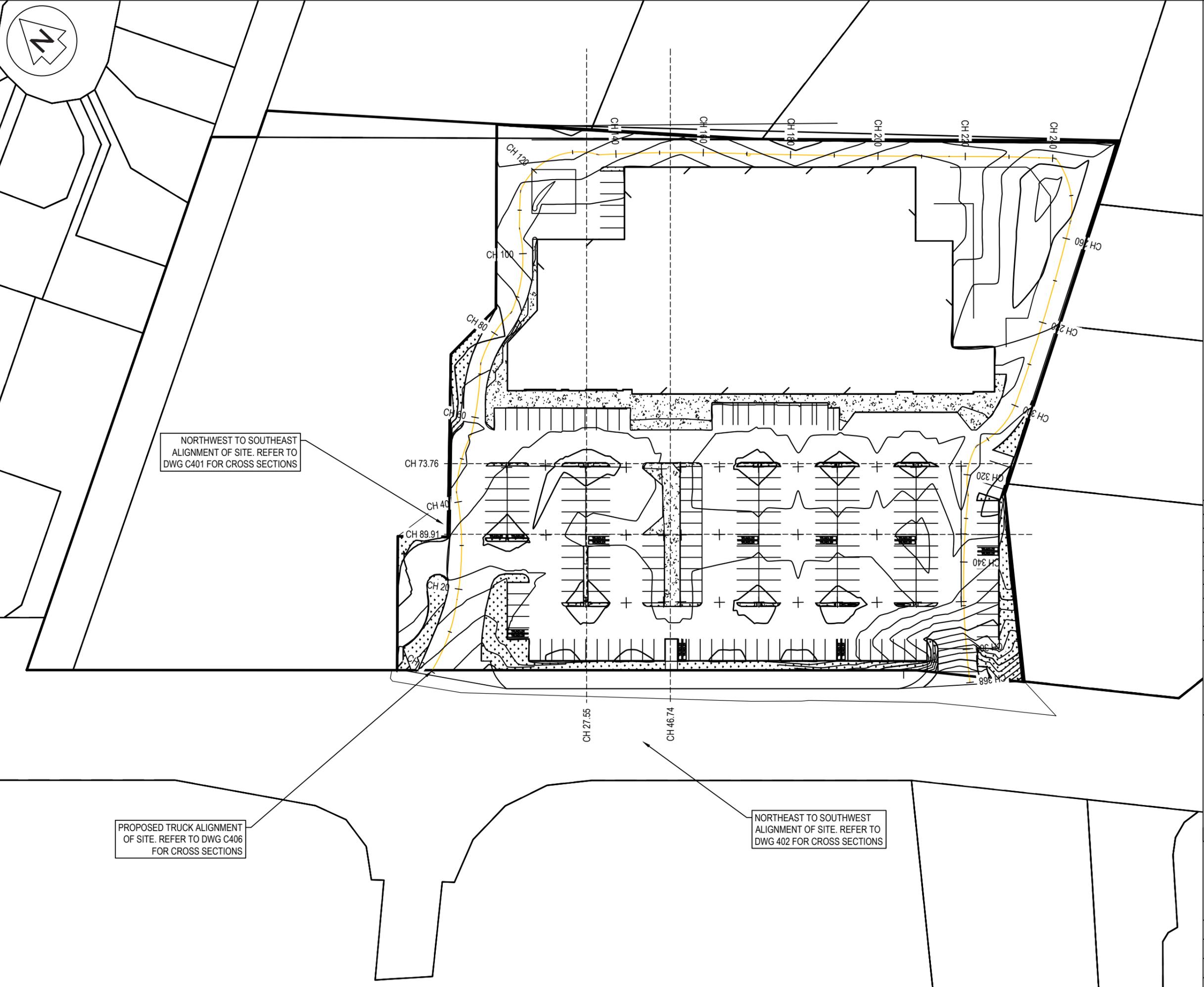
Rev	Description	By	Date
A	RC	CA	06/2021

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Project
160
KAPITI ROAD
FOR
KAPITI RETAIL
HOLDINGS LTD

Title
PROPOSED
CARPARK
CROSS SECTIONS

Project no.	109022
Scale	1:250 @ A3
Cad file	109022 C300 SITE PLAN.DWG
Drawing no.	C320
Rev	A



- Notes
1. All works to be in accordance with KCDP council standards.
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 4. The contractor shall obtain all necessary approval from utility operators before commencing work under or near their services.

Legend

— EX BDY

- - - PROP BDY

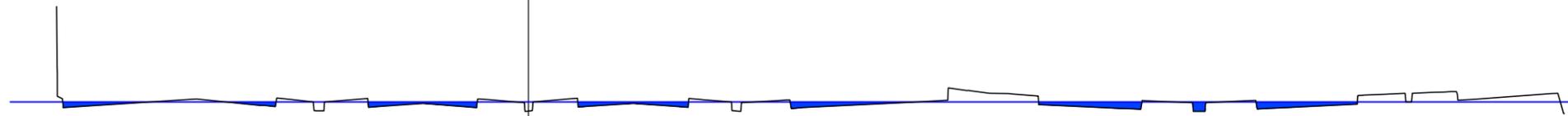
A	DRAFT	MA	05/2021
Rev	Description	By	Date
Survey	BY	DATE	
Design	BY	DATE	
Drawn	MA	05/2021	
Checked	BY	DATE	

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Project
**160 KAPITI ROAD
 PARAPARAUMU
 FOR
 KAPITI RETAIL HOLDINGS
 LIMITED**

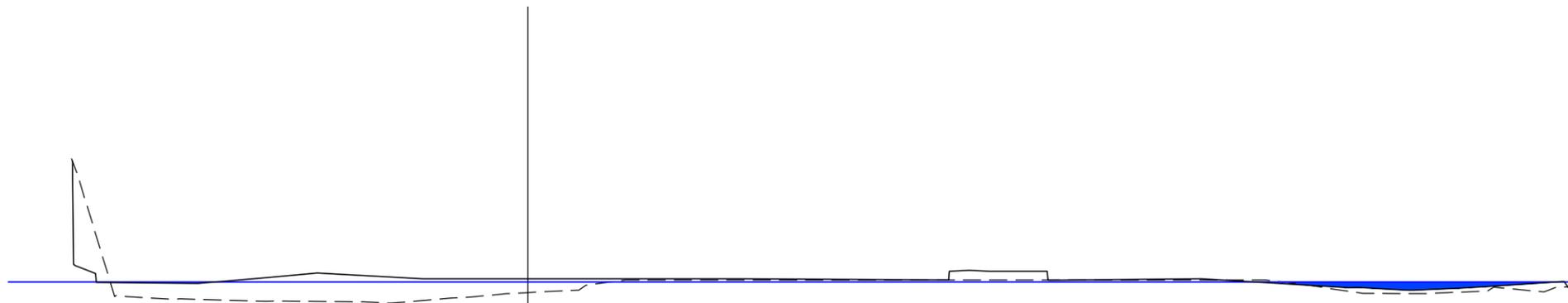
Title
**SITE
 OVERVIEW
 PLAN**

Project no.	109022
Scale	NTS
Cad file	109022 C400 COPY.DWG
Drawing no.	C400
Rev	A



	DATUMN 4.00																										
EXISTING LEVELS	7.83	6.01	5.90	5.90	5.91	5.85	5.85	5.91	5.96	6.05	6.13	6.13	6.13	6.13	6.13	6.13	6.13	6.13	6.13	6.13	6.09	5.99	5.93	5.94	5.94		
DESIGN LEVELS	6.14	6.07	6.13	6.08	6.14	6.14	6.06	6.01	5.94	6.02	6.06	6.14	6.11	6.02	6.07	6.12	6.25	6.04	6.00	6.11	6.11	6.00	6.05	6.25	6.13	6.20	
OFFSETS	-40.00	-35.00	-30.00	-25.00	-20.00	-15.00	-10.00	-5.00	0.00	5.00	10.00	15.00	20.00	25.00	30.00	35.00	40.00	45.00	50.00	55.00	60.00	65.00	70.00	75.00	80.00	85.00	90.00

CH 73.76m



	DATUMN 4.00																										
EXISTING LEVELS		5.87	5.81	5.78	5.77	5.76	5.77	5.84	5.92	6.04	6.13	6.13	6.13	6.13	6.13	6.13	6.13	6.13	6.13	6.13	6.09	5.95	5.90	5.93	5.97	6.01	
DESIGN LEVELS		6.09	6.08	6.13	6.22	6.22	6.16	6.15	6.15	6.15	6.15	6.15	6.15	6.15	6.14	6.13	6.28	6.13	6.14	6.15	6.13	6.06	6.00	5.96	6.00	6.06	
OFFSETS	-40.00	-35.00	-30.00	-25.00	-20.00	-15.00	-10.00	-5.00	0.00	5.00	10.00	15.00	20.00	25.00	30.00	35.00	40.00	45.00	50.00	55.00	60.00	65.00	70.00	75.00	80.00	85.00	90.00

CH 89.91m

Legend
 - - - - EX LEVELS
 ——— DESIGN LEVELS
 ——— 100 YEAR FLOOD LEVEL
 ■■■■■ FLOOD VOLUME

Rev	Description	By	Date
A	DRAFT	MA	05/2021
Survey	BY	DATE	
Design	BY	DATE	
Drawn	MA	05/2021	
Checked	BY	DATE	

M Maven Associates
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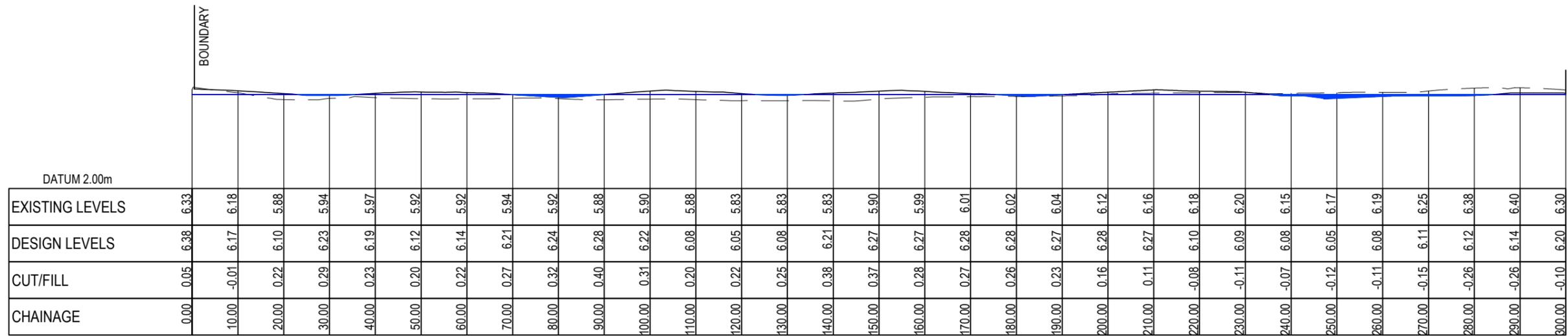
Project
**160 KAPITI ROAD
 PARAPARAUMU
 FOR
 KAPITI RETAIL HOLDINGS
 LIMITED**

Title
**STORMWATER
 FLOODING
 CROSS SECTIONS**

Project no.	109022
Scale	1:1000 H 1:200 V @ A3
Cad file	109022 C400 COPY.DWG
Drawing no.	C401
Rev	A

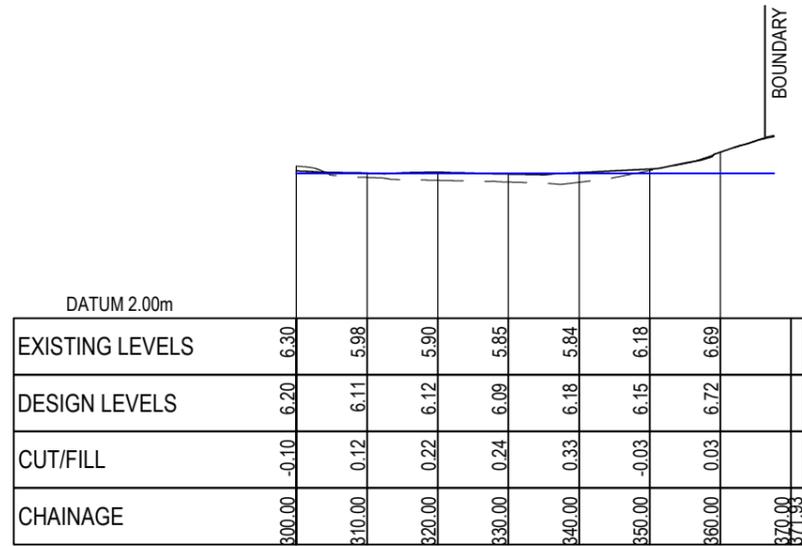
NORTHWEST TO SOUTHEAST OF SITE CROSS SECTIONS

SCALE 1:1000 HORI 1:200 VERT @ A3



PROPOSED TRUCK ACCESS PATH (1 OF 2)
SCALE: HORI 1:1250 VERT 1:250

Legend
 - - - - - EX LEVELS
 ——— DESIGN LEVELS
 ——— 100 YEAR FLOOD LEVEL RL 6.10m



PROPOSED TRUCK ACCESS PATH (2 OF 2)
SCALE: HORI 1:1250 VERT 1:250

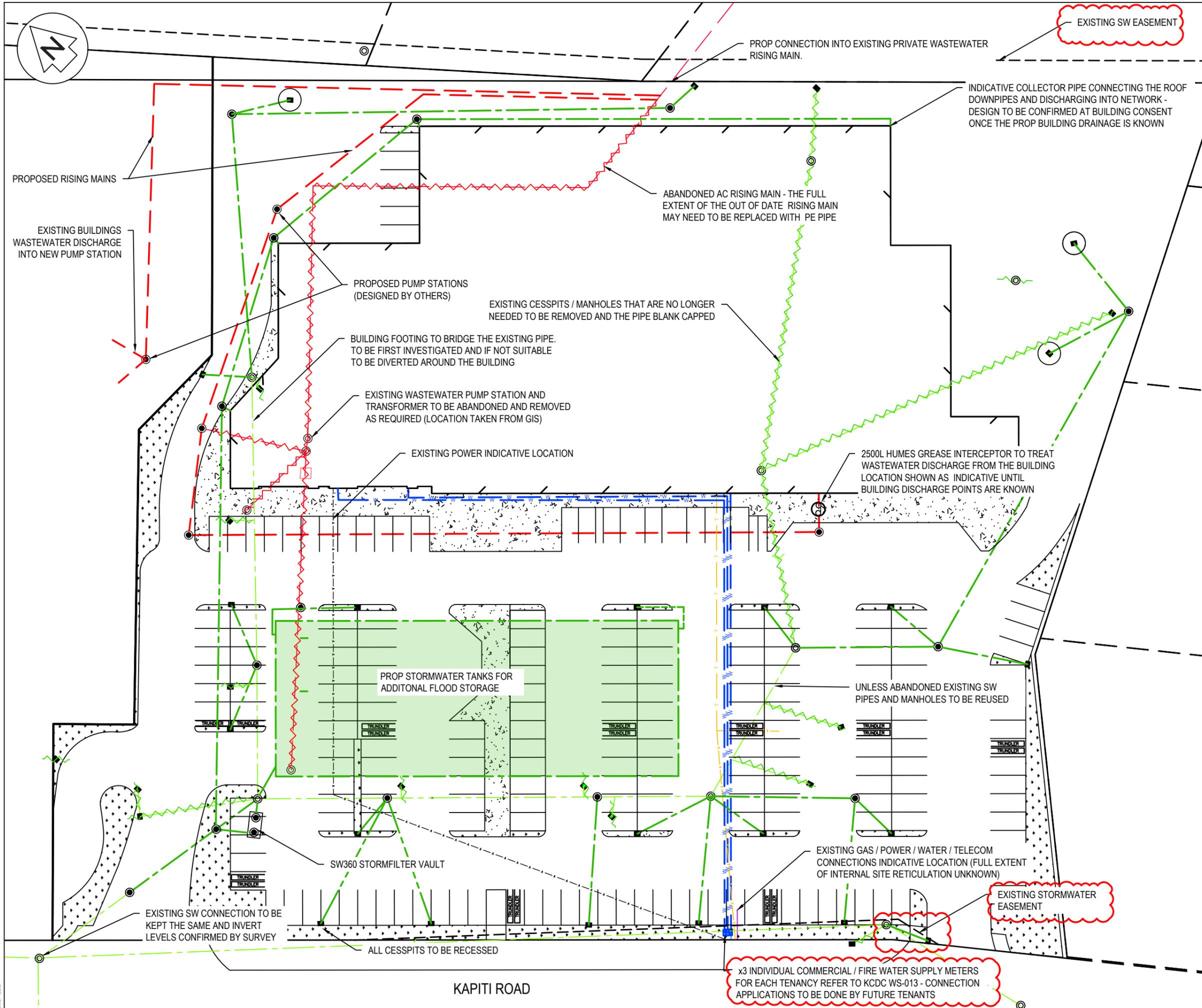
Rev	Description	By	Date
A	DRAFT	MA	05/2021

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Project
**160 KAPITI ROAD
 PARAPARAUMU
 FOR
 KAPITI RETAIL HOLDINGS
 LIMITED**

Title
**TRUCK
 ACCESS
 LONG SECTION**

Project no.	109022
Scale	1:1000 H 1:200 V @ A3
Cad file	109022 C400 COPY.DWG
Drawing no.	C403
Rev	A



- Notes
1. All bends and connections to be no more than 45°
 2. All connections to existing drains shall be carried out by a licensed Drainlayer/Plumber.
 3. Drainage shall comply in full with E1/AS1 building code for storm water.
 4. All cesspits shall have half syphons installed.
 5. All sanitary waste drains shall be uPVC to AS/NZS 1260.
 6. Sewer shall comply in full with AS/NZS 3500.2 - 2003 and/or G13 Building Code
 7. All pipes shall be SN16 grade unless otherwise stated.
 8. Drainlayer shall locate and confirm connection invert before starting building works.
 9. Plans to be read in conjunction with Hydraulic Engineers and differences shall be clarified before contractor starts.
 10. All chamber lids shall have a minimum 200mm maximum 300 throat to provide sufficient cover for landscape and pavement over the top.
 11. Building discharge points are indicative to be confirmed at building consent once locations are confirmed by the architect

Legend

---	EX BDY
---	ABUTTING BDY'S
---	PROP/EX EASEMENT
---	PROP BDY
---	PROP BLDG PLATFORM
---	EX STORMWATER
---	EX WASTEWATER
---	PR STORMWATER
---	PR WASTEWATER
---	EX WATERMAIN
---	PROP WATERMAIN
---	EX TELECOM
---	EX POWER
---	EX GAS
---	PROP TELECOM
---	PROP POWER
---	PROP GAS
---	EX/PROP SWMH
---	PROP SWCP SINGLE
---	ABANDONED WW
---	ABANDONED SW
---	FLOOD STORAGE TANKS

B	RC	CA	08/2021
A	RC	CA	05/2021
Rev	Description	By	Date

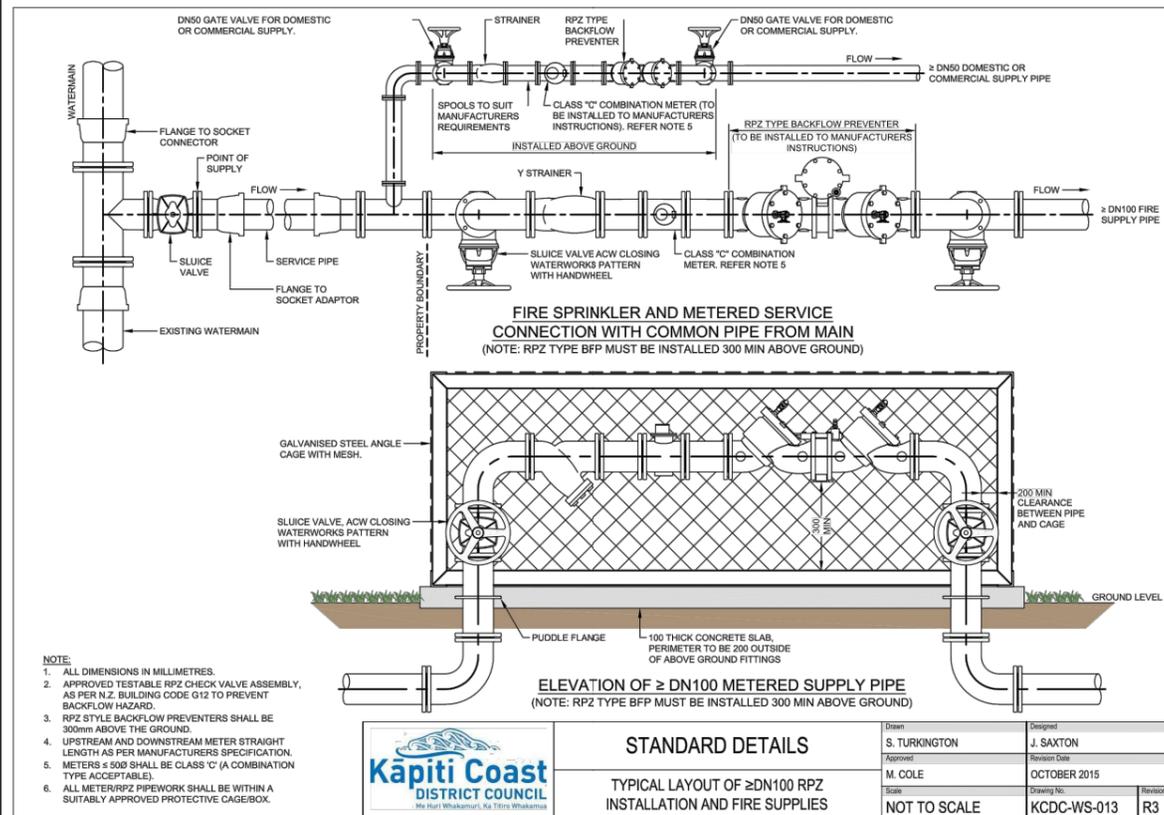
M **Maven Associates**
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 Auckland 1023

Project
160
KAPITI ROAD
FOR
KAPITI RETAIL
HOLDINGS LTD

Title
PROPOSED PRIVATE
SERVICES
PLAN

Project no.	109022
Scale	1:500 @ A3
Cad file	109022 C410.DWG
Drawing no.	C410
Rev	B

DATE: 8/2/21



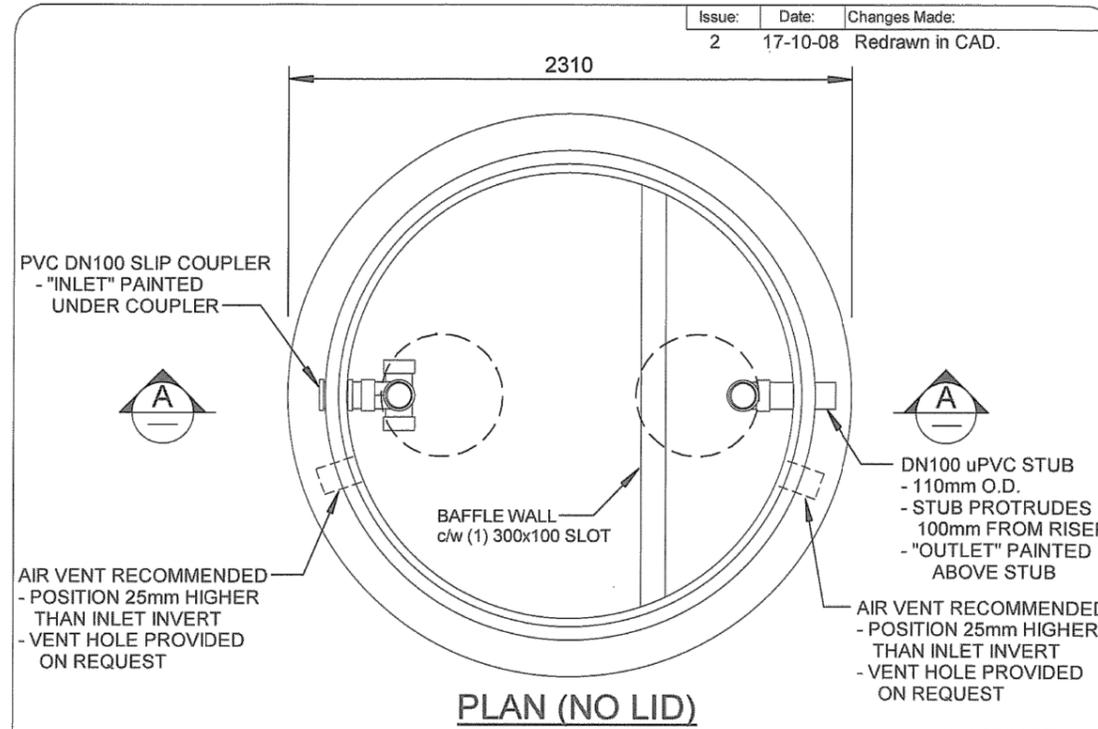
- NOTE:**
1. ALL DIMENSIONS IN MILLIMETRES.
 2. APPROVED TESTABLE RPZ CHECK VALVE ASSEMBLY, AS PER N.Z. BUILDING CODE G12 TO PREVENT BACKFLOW HAZARD.
 3. RPZ STYLE BACKFLOW PREVENTERS SHALL BE 300mm ABOVE THE GROUND.
 4. UPSTREAM AND DOWNSTREAM METER STRAIGHT LENGTH AS PER MANUFACTURERS SPECIFICATION.
 5. METERS ≤ 500 SHALL BE CLASS 'C' (A COMBINATION TYPE ACCEPTABLE).
 6. ALL METER/RPZ PIPEWORK SHALL BE WITHIN A SUITABLY APPROVED PROTECTIVE CAGE/BOX.



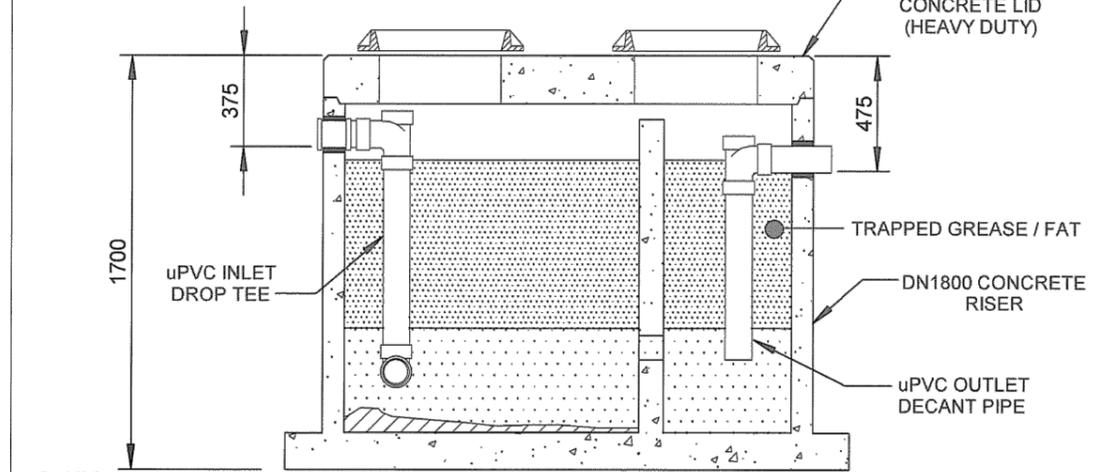
STANDARD DETAILS
TYPICAL LAYOUT OF ≥DN100 RPZ INSTALLATION AND FIRE SUPPLIES

Drawn	S. TURKINGTON	Designed	J. SAXTON
Approved	M. COLE	Revision Date	OCTOBER 2015
Scale	NOT TO SCALE	Drawing No.	KCDC-WS-013
		Revision	R3

Original Sheet Size A3 [297x420]



PLAN (NO LID)



SECTION A-A

NOTES:

1. SWIFTLIFT LIFTING ANCHORS PROVIDED ON ALL COMPONENTS
2. TOTAL MASS: 5.68 TONNES
BODY: 4.10 TONNES
CONC LID: 1.41 TONNES (INCL. CAST IRON COVERS & FRAMES)
3. LID DESIGN - HEAVY DUTY, 0.85HN VEHICLE LOADS
4. CAPACITY - 2,706 LITRES, DEFINED AS TANK VOLUME BELOW INVERT LEVEL OF OUTLET PIPE
5. GREASE STORAGE VOLUME - 2,187 LITRES, S.G. = 0.9



Humes Pipeline Systems
Private Bag 92817
Auckland
Freephone 0800 502 112
www.humes.co.nz

**GREASE INTERCEPTOR
MODEL GT2500
(HEAVY DUTY)
GENERAL ASSEMBLY DETAILS**

Drawn By:	C THORPE	Date:	02-10-07
Checked By:		Scale:	1 : 25
Reference:			
Drawing No.:	HUMES 3031	Issue:	2

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Issue:	Date:	Changes Made:
2	17-10-08	Redrawn in CAD.

Rev	Description	By	Date
A	RC	CA	05/2021
Survey			
Design	CA		05/2021
Drawn	CA		05/2021
Checked	BB		05/2021

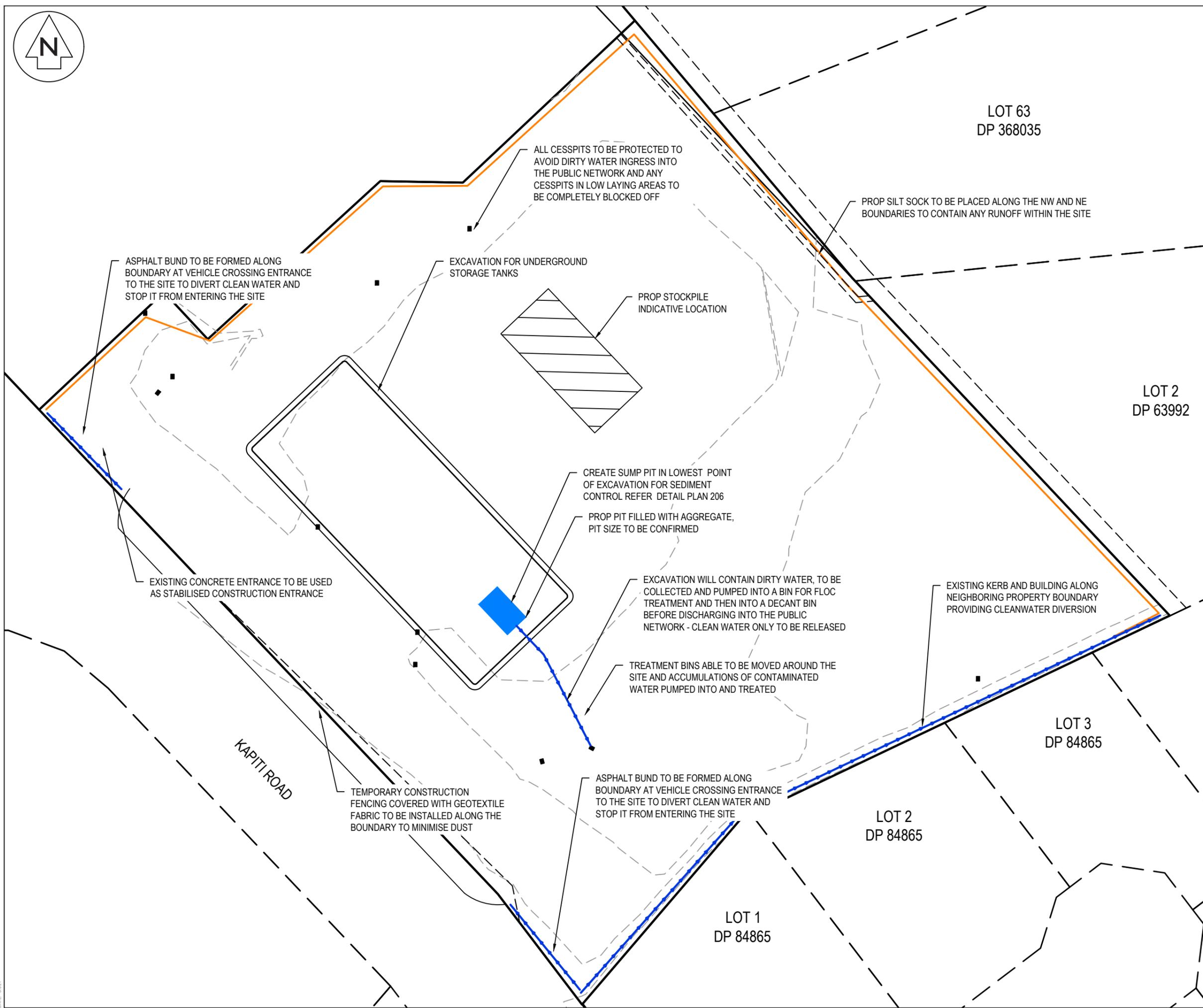


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Project
160
**KAPITI ROAD
FOR
KAPITI RETAIL
HOLDINGS LTD**

Title
**PROPOSED
SERVICES STANDARD
DETAILS**

Project no.	109022
Scale	NTS
Cad file	109022 C410.DWG
Drawing no.	C490
Rev	A



- Notes
1. All works to be in accordance with KCDP council standards.
 2. It is the contractors responsibility to locate all services that may be affected by his operations.
 3. The contractor shall comply with all relevant Health and Safety requirements.
 4. The contractor shall obtain all necessary approval from utility operators before commencing work under or near their services.
 5. Sediment control shall be installed and operational before earthworks start onsite in accordance with council standards.
 6. Contractor shall provide asbuilt of working sediment control devices and confirmation of pond/decent volumes to engineer.

Legend

	EX BDY
	PROP BDY
	EX ABUTTING BDY
	PROP/EX EASEMENT
	EX MAJOR CONTOUR
	PR MAJOR CONTOUR
	PR MINOR CONTOUR
	PROP SILT SOCK
	PROP STOCKPILE
	PROP CLEANWATER
	EX CESSPIT

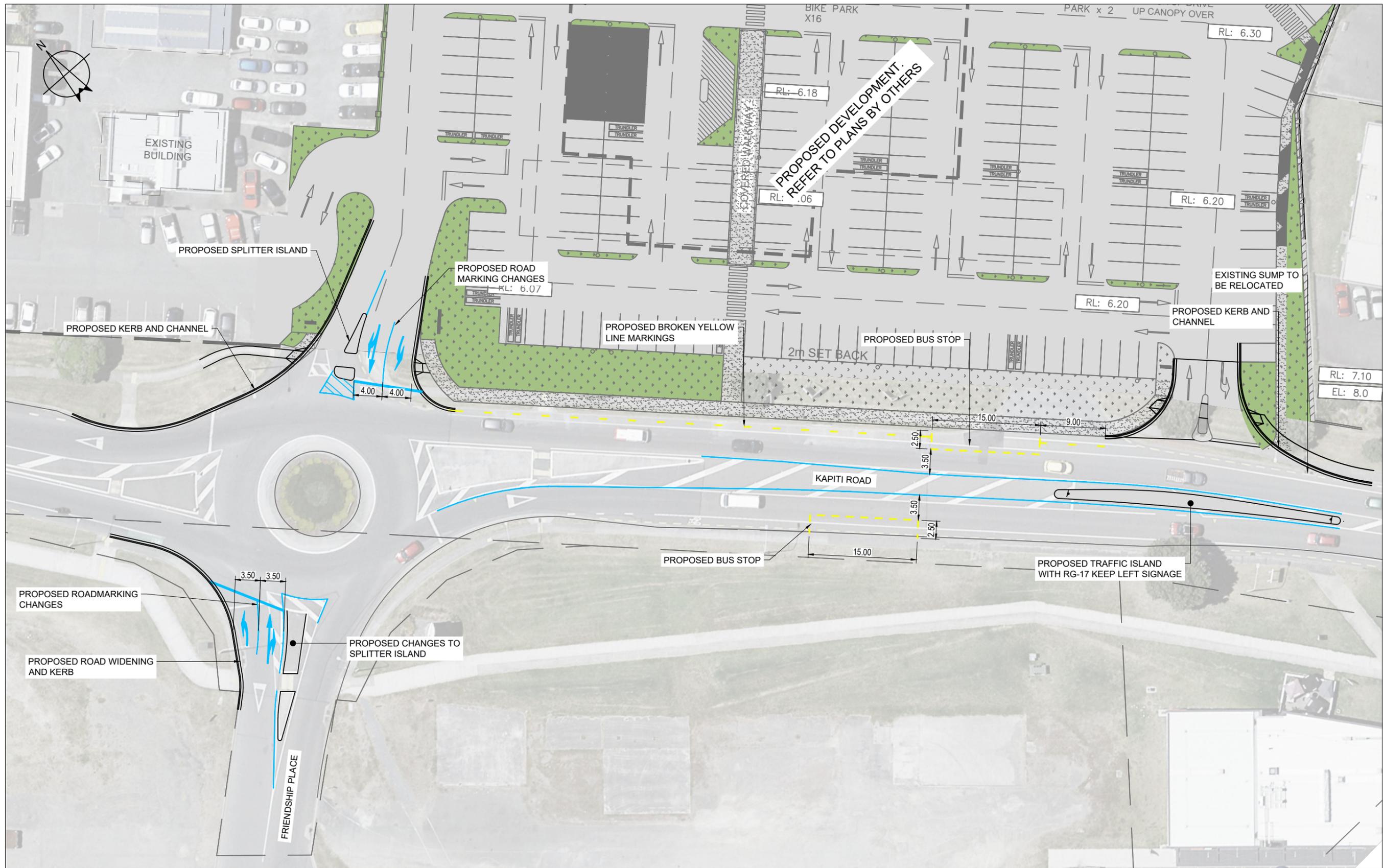
Rev	Description	By	Date
A	RC	CA	08/2021
Survey	CUTTRISS		09/16
Design	CA		06/21
Drawn	CA		06/21
Checked	GB		06/21

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 Auckland 1023

Project
**160 KAPITI ROAD
 PARAPARAUMU
 FOR
 KAPITI RETAIL HOLDINGS
 LIMITED**

Title
**PROPOSED
 SEDIMENT CONTROL
 PLAN**

Project no.	109022
Scale	NTS
Cad file	109022 C200 PONDING.DWG
Drawing no.	C205
Rev	A



PROPOSED DEVELOPMENT.
REFER TO PLANS BY OTHERS

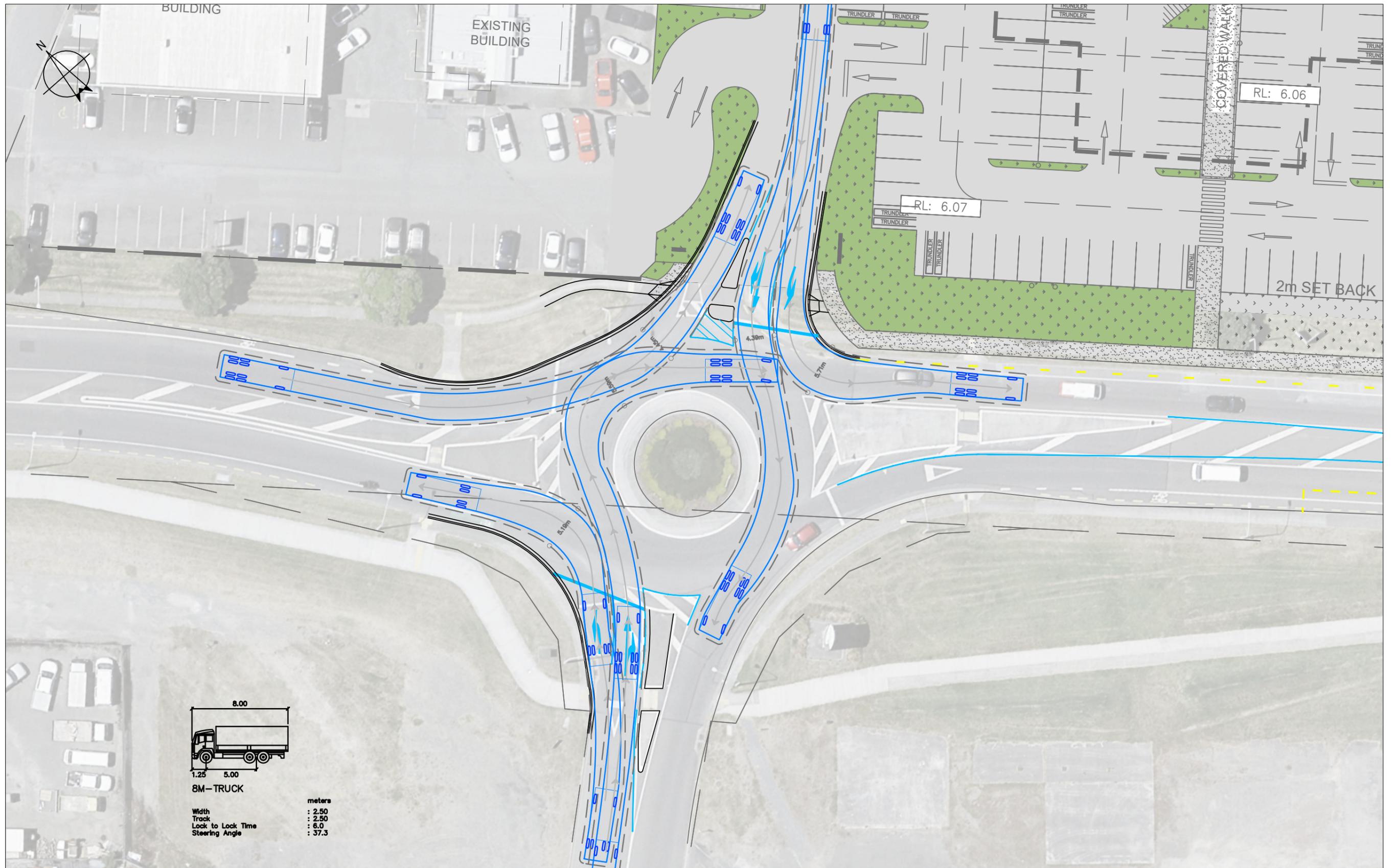
NOT FOR CONSTRUCTION

REV	REVISION DESCRIPTION	DRAWN	CHECKED	APPROVED	DATE	APPROVED	NAME
D	DESIGN REVISED TO AVOID LAND TAKE	JO	TK		03.12.21	DESIGNED	J. ORRINGE
C	LAND TAKE AREA ADDED	JO	TK		01.12.21	DRAWN	J. ORRINGE
B	23m B-TRAIN TRACKING ADDED	JO	TK		29.09.21	REVIEWED	T. KELLY
A	ISSUED FOR DISCUSSION	JO	TK		28.09.21	APPROVED	

CTD
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josh@ctdesign.co.nz
027 641 6653

TIM KELLY TRANSPORTATION PLANNING LIMITED
COUNTDOWN KAPITI ROAD, PARAPARAUMU
INDICATIVE ROAD LAYOUT CHANGES

DRAWING STATUS: CONCEPT	
PLOT DATE: 03.12.2021	SCALE AT A1: 1:250
DRAWING NO. 21-005-SK001	REV C



NOT FOR CONSTRUCTION

REV	REVISION DESCRIPTION	DRAWN	CHECKED	APPROVED	DATE
D	DESIGN REVISED TO AVOID LAND TAKE	JO	TK		03.12.21
C	LAND TAKE AREA ADDED	JO	TK		01.12.21
B	23m B-TRAIN TRACKING ADDED	JO	TK		29.09.21
A	ISSUED FOR DISCUSSION	JO	TK		28.09.21

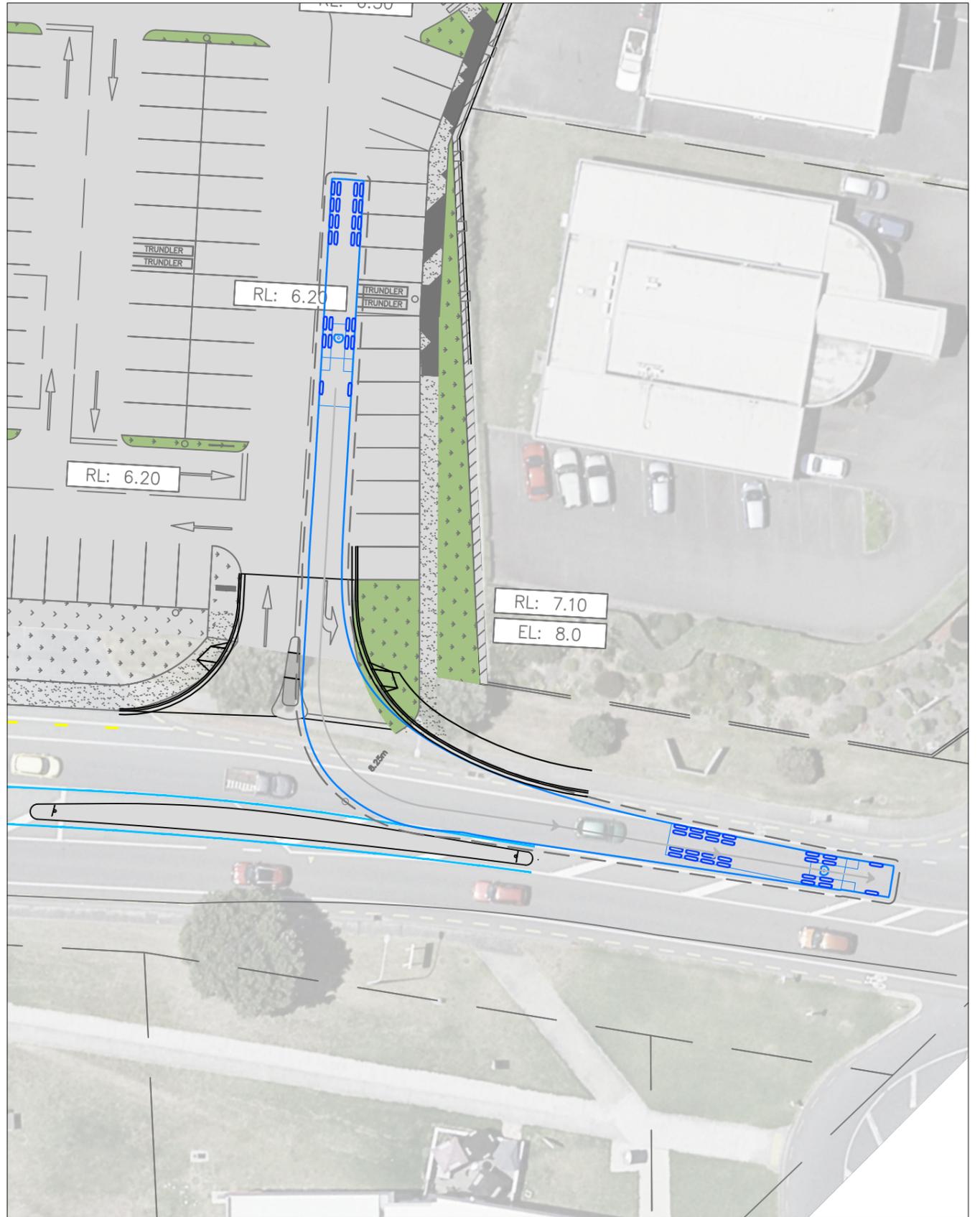
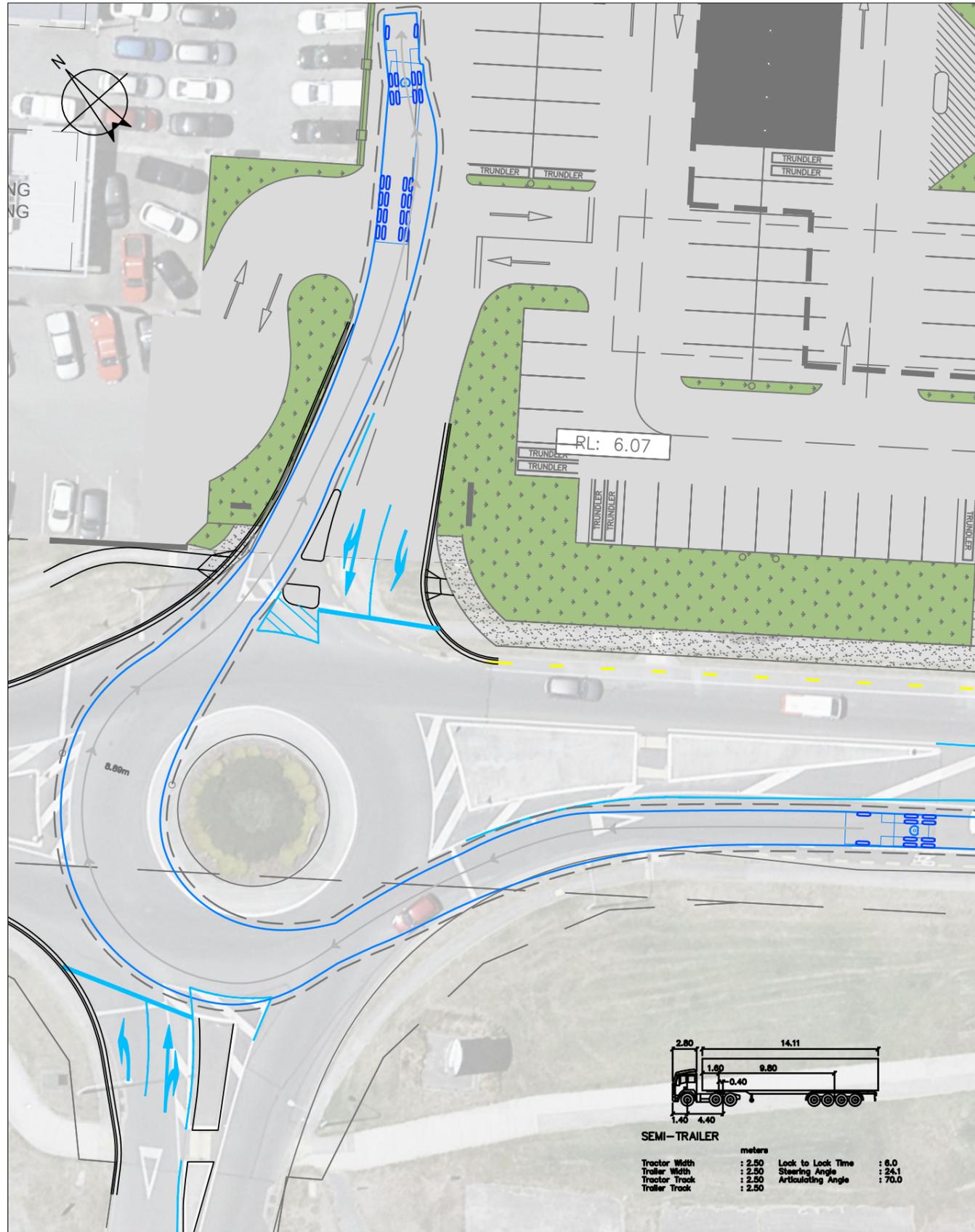
	NAME
DESIGNED	J. ORRINGE
DRAWN	J. ORRINGE
REVIEWED	T. KELLY
APPROVED	

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 josh@ctdesign.co.nz
 027 641 6653

TIM KELLY TRANSPORTATION PLANNING LIMITED
 COUNTDOWN KAPITI ROAD, PARAPARAUMU

VEHICLE TRACKING PLAN
 8m MEDIUM RIGID TRUCK

DRAWING STATUS:	
CONCEPT	
PLOT DATE: 03.12.2021	SCALE AT A1: 1:200
DRAWING NO. 21-005-SK002	REV C



NOT FOR CONSTRUCTION

REV	REVISION DESCRIPTION	DRAWN	CHECKED	APPROVED	DATE
D	DESIGN REVISED TO AVOID LAND TAKE	JO	TK		03.12.21
C	LAND TAKE AREA ADDED	JO	TK		01.12.21
B	23m B-TRAIN TRACKING ADDED	JO	TK		29.09.21
A	ISSUED FOR DISCUSSION	JO	TK		28.09.21

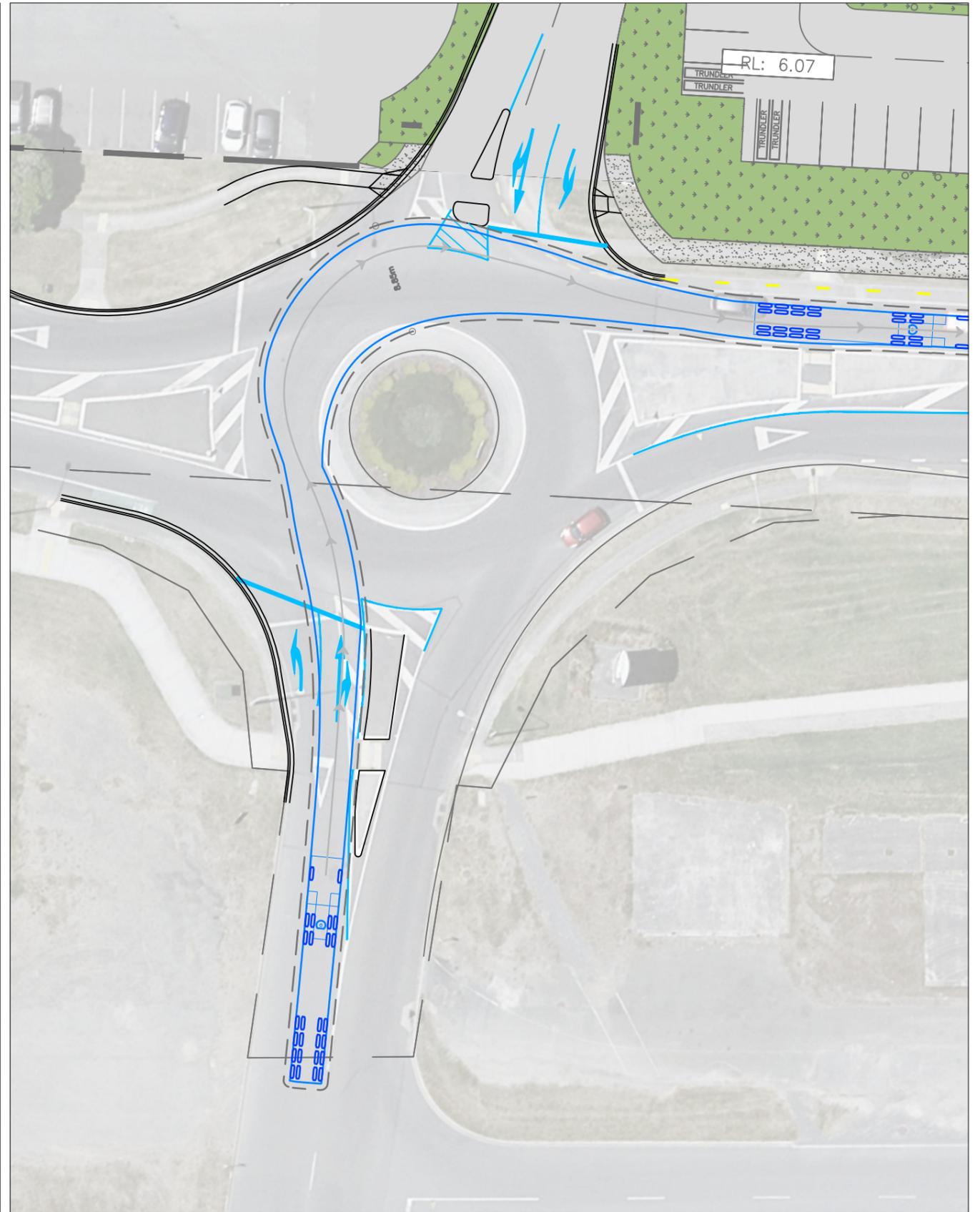
NAME	ROLE
J. ORRINGE	DESIGNED
J. ORRINGE	DRAWN
T. KELLY	REVIEWED
	APPROVED



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TIM KELLY TRANSPORTATION PLANNING LIMITED
 COUNTDOWN KAPITI ROAD, PARAPARAUMU
 VEHICLE TRACKING PLAN
 SEMI - TRAILER TRACKING

DRAWING STATUS:	
CONCEPT	
PLOT DATE: 03.12.2021	SCALE AT A1: 1:200
DRAWING NO. 21-005-SK003	REV C



NOT FOR CONSTRUCTION

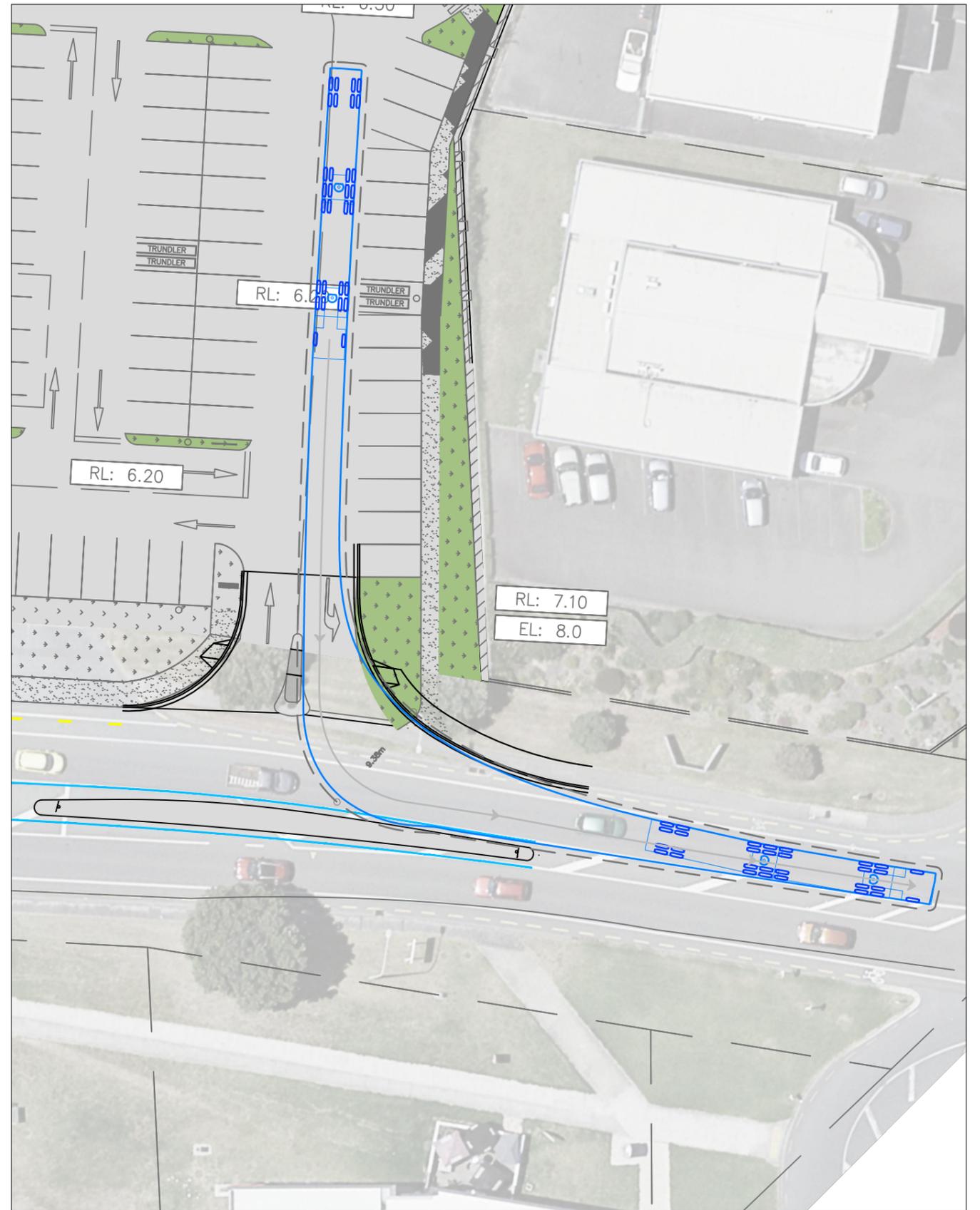
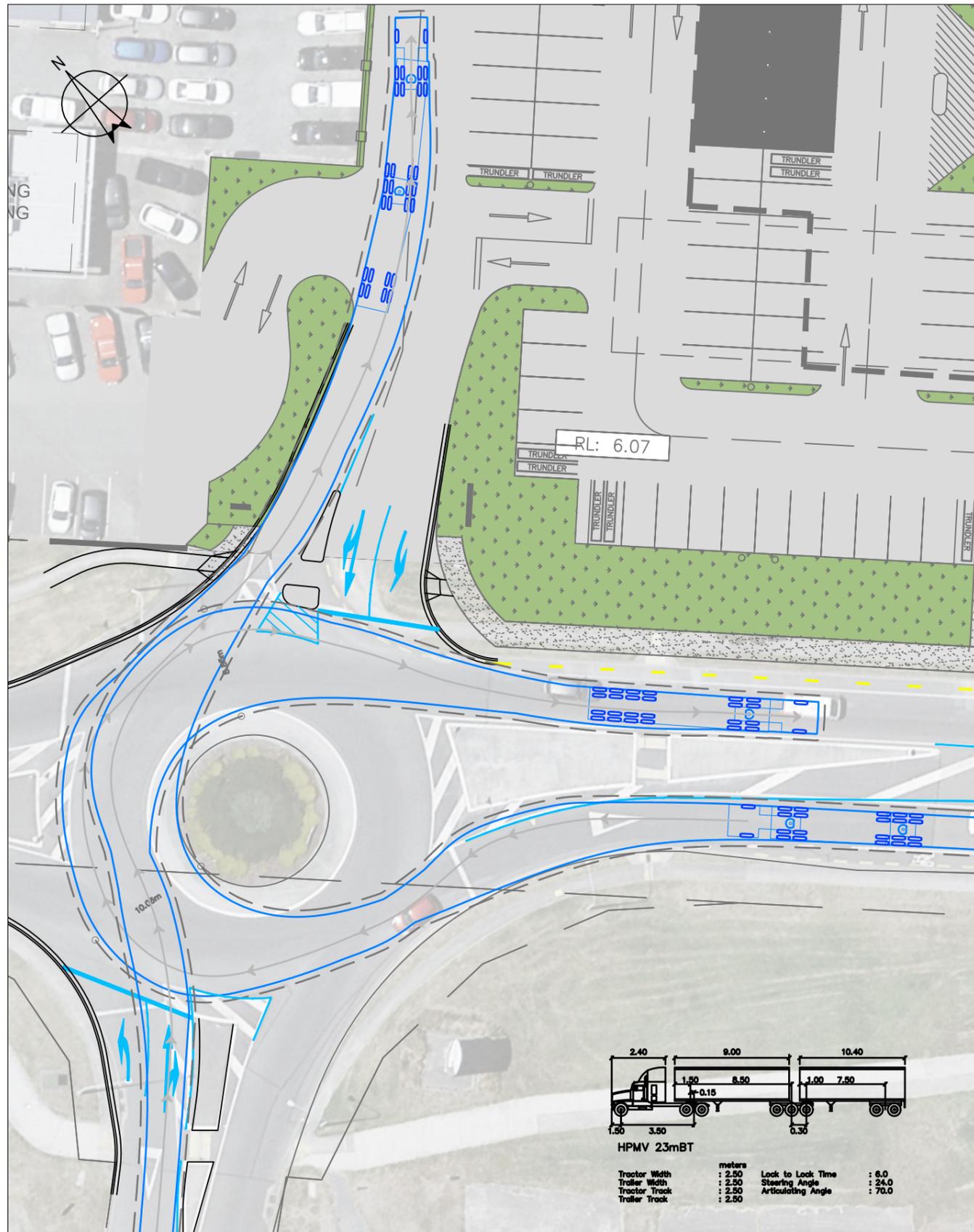
REV	REVISION DESCRIPTION	DRAWN	CHECKED	APPROVED	DATE
D	DESIGN REVISED TO AVOID LAND TAKE	JO	TK		03.12.21
C	LAND TAKE AREA ADDED	JO	TK		01.12.21
B	23m B-TRAIN TRACKING ADDED	JO	TK		29.09.21
A	ISSUED FOR DISCUSSION	JO	TK		28.09.21

NAME	DESIGNED	DRAWN	REVIEWED	APPROVED
J. ORRINGE	J. ORRINGE	T. KELLY		

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josh@ctdesign.co.nz
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TIM KELLY TRANSPORTATION PLANNING LIMITED
COUNTDOWN KAPITI ROAD, PARAPARAUMU
VEHICLE TRACKING PLAN
SEMI TRAILER

DRAWING STATUS: CONCEPT	
PLOT DATE: 03.12.2021	SCALE AT A1: 1:200
DRAWING NO. 21-005-SK004	REV C



NOT FOR CONSTRUCTION

REV	REVISION DESCRIPTION	DRAWN	CHECKED	APPROVED	DATE
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C	LAND TAKE AREA ADDED	JO	TK		01.12.21
B	23m B-TRAIN TRACKING ADDED	JO	TK		29.09.21
A	ISSUED FOR DISCUSSION	JO	TK		28.09.21

NAME	ROLE
J. ORRIDGE	DESIGNED
J. ORRIDGE	DRAWN
T. KELLY	REVIEWED
	APPROVED

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TIM KELLY TRANSPORTATION PLANNING LIMITED
COUNTDOWN KAPITI ROAD, PARAPARAUMU

VEHICLE TRACKING PLAN
23m B-TRAIN

DRAWING STATUS:	
CONCEPT	
PLOT DATE: 03.12.2021	SCALE AT A1: 1:200
DRAWING NO. 21-005-SK005	REV C

**POST OR EMAIL THIS COMPLETED PAGE AT LEAST 48 HOURS BEFORE
CONSTRUCTION TAKES PLACE**

TO:

The RMA Compliance Officer
Kāpiti Coast District Council
Private Bag 60601
PARAPARAUMU
Email: compliance.dutyofficer@kapiticoast.govt.nz

Application No:	RM210151
Site Address:	160 Kapiti Road, Paraparaumu Lots 1 & 2 DP 63027 and Lot 3 DP 63992
Proposal:	Undertake earthworks on a contaminated site for the construction of a building to be used for a retail activity (supermarket) that does not comply with the permitted activity standards for landscaping; signage; vehicle movement; and, access.

Construction will take place on (date):
--

Owner's Contact Details	
Name:	
Postal Address:	
Telephone:	Mobile:

Contractor's Contact Details (e.g. earthworks)	
Name:	
Telephone:	Mobile:

SIGNED:

.....
NAME

.....
DATE